

Downtown Porterville

DESIGN GUIDELINES

Adopted April 20, 2010
Resolution 48-2010

Downtown Porterville

DESIGN GUIDELINES

Prepared by

DYETT & BHATIA

Urban and Regional Planners

Adopted April 20, 2010

This page intentionally left blank

TABLE OF CONTENTS

Introduction.....	1
Downtown Districts Map.....	4
A. Building Form and Building to Street Relationship.....	5
1. Downtown Retail –North of Olive Avenue (DR-N)	5
2. Downtown Retail –South of Olive Avenue (DR-S)	13
3. Downtown Retail – D Street Corridor (DR-D).....	19
4. Downtown Mixed Use (D-MX).....	23
5. Downtown Professional Office (D-PO)	29
6. Downtown Public and Semi-Public (D-PS)	36
7. Downtown Medium Density Residential (DRM-2).....	39
8. Downtown High Density Residential (DRM-3)	41
9. Downtown General Commercial (D-GC)	45
B. Building Design	49
1. Exterior Building Materials and Colors	49
2. Ground Floor Windows and Design.....	51
3. Limits on Blank Walls	52
4. Commercial Ground Floor Articulation.....	53
5. Building Projections	54
6. Residential Building Articulation.....	55
7. Building Entrances	56
8. Shade.....	58
9. Awnings and Building Security Devices.....	59
10. Utilities Screening	61
11. Rooftop Mechanical Equipment Screening.....	61
C. Parking	63
D. Signs	65
E. Street Improvements	67
F. Glossary.....	71

LIST OF FIGURES

Downtown Districts Map	4
A1-1: Downtown Retail North: Building Form	6
A1-2: Downtown Retail North: Building Form and Site Planning	7
A1-3: Downtown Retail North: Main Street.....	9
A1-4: Downtown Retail North: New Construction Guidelines for Period Architecture.....	10
A1-5: Downtown Retail North: Facade Renovation Guidelines	11
A1-6: Downtown Retail North: Facade Renovation Guidelines.....	12
A2-1: Downtown Retail South: Building Form	14
A2-2: Downtown Retail South: Building Form and Site Planning.....	15
A2-3: Downtown Retail South: Orange Street Enhancements	17
A2-4: Downtown Retail South: Main Street Enhancements	18
A2-5: Downtown Retail South: Other Street Enhancements (C Street_.....	18
A3-1: Downtown Retail D Street: Building Form	20
A3-2: Downtown Retail D Street: Building Form and Site Planning.....	21
A3-3: Upper D Street Enhancements	22
A3-4: D Street Shopping Centers	22
A4-1: Downtown Mixed Use: Building Form	24
A4-2: Downtown Mixed Use: Building Form and Site Planning	25
A4-3: Downtown Mixed Use Large Site Building Form and Site Planning.....	27
A4-4: Downtown Mixed Use: 3rd and 4th Street Enhancements	28
A4-5: Downtown Mixed Use: Main Street Enhancements.....	28
A5-1: Downtown Professional Office: Building Form - Renovations and Additions.....	30
A5-2: Downtown Professional Office: Building Form - New Construction	31
A5-3: Downtown Professional Office: D Street Enhancements	33
A5-4: 2nd Street Enhancements.....	33
A5-5: Downtown Professional Office: Putnam Street Enhancements	34
A5-6: Downtown Professional Office: Thurman & Cleveland Street Enhancements	35
A6-1: Downtown Public and Semi-Public: Building Form.....	37
A6-2: Downtown Public and Semi-Public: Building to Street Relationship on Olive and Plano	38
A7-1: Downtown Medium Density Residential: Building Form	40
A8-1: Downtown High Density Residential: Building Form	42
A8-2: Downtown High Density Residential: Building Form and Site Planning.....	43

A8-3: Typical Street Enhancements (B Street)	44
A9-1: Downtown General Commercial: Building Form	46
A9-2: Hockett Street Enhancements	47
B1-1: Quality Materials	49
B1-2: Building Base	49
B1-3: Wall Surface - Inappropriate	50
B1-4: Colors - Inappropriate	50
B2-1: Ground Floor Windows and Design	51
B3-1: Limits on Blank Walls	52
B4-1: Ground Floor Articulation	53
B5-1: Building Projections	54
B7-1: Commercial Entrances	56
B7-2: Mixed Use Entrances	56
B7-3: Residential Entrances	57
B8-1: Shade	58
B9-1: Awnings and Canopies	60
C-1: Parking Pot Landscaping	63
C-2: On-Site Pedestrian Walkways	64
D-1: Front Yard Signs	65
D-2: Window Sign	65
D-3: Front Facade Upper Transom Signs	65
D-4: Rear and Side Facade Signs	65
D-5: Rear and Side Facade Signs	66
D-6: Monument Signs	66
D-7: Projecting Blade Signs	66
D-8: Guild Signs	66
E-1: Pedestrian Amenities	67
E-2: Street Preservation	68

This page intentionally left blank

INTRODUCTION

PURPOSE OF THE DESIGN GUIDELINES

Downtown is a special place in Porterville, with a distinctive character. The City has long-established policies about enhancing the appearance and vitality of Downtown, and has invested substantial time and money into improving this historic center of the City. The Design Guidelines describe and illustrate the design criteria for projects on Downtown properties. These guidelines are consistent with the General Plan and the Development Ordinance, but are more qualitative in nature. The purpose of the guidelines is to:

- Guide property owners, developers, and architects in the preparation of development applications;
- Guide City staff and the City Council in the review of Downtown project applications, by establishing the review criteria; and
- Guide property owners, businesses, and residents as to development standards and guidelines for their property, surrounding properties, and other areas of Downtown.

DOWNTOWN GOALS

The Downtown Design Guidelines have been prepared in order to achieve the goals for Downtown, as established in the City's General Plan. Those goals are:

1. Maintain a thriving, vibrant Downtown compatible with surrounding land uses.
2. Foster the location of cultural and civic uses and public open space to maintain Downtown as the civic core of the City.
3. Provide for a range of commercial and retail services that add to a diversified economic base.
4. Encourage mixed uses to create a more vibrant community and offer additional opportunities for housing for residents requiring convenient access to community services and less dependency on auto transportation.
5. Ensure that development is attractive and pedestrian friendly.
6. Preserve and enhance the pedestrian-oriented environment of the Downtown area.
7. Ensure that potential development and redevelopment is integrated into the Downtown and supports the vitality of the area.

DESIGN GUIDELINES CONTENT AND ORGANIZATION

The Design Guidelines consist primarily of illustrations, accompanied by text and annotations. They present key concepts related to site planning and building design. However, they do not illustrate architectural designs, and should not be taken literally as a prescribed design solution. Each property owner, business, and resident should evaluate their individual property and figure out how best to comply with the guidelines, while at the same time meeting their own needs and desires. The guidelines are intended to provide flexibility and accommodate many different land uses, designs, and building styles.

The guidelines are organized into five sections, in alphabetical order. **Section A** presents the guidelines for building form and the building to street relationship for each of the nine districts within Downtown, based on the character of the streets, land uses, and building forms. For each district there is a set of standards for density and building height, guidelines about building form and street design, and graphics illustrating the key site planning and building form guidelines. There are also street design diagrams that recommend streetscape enhancements to make streets more attractive, and add trees and shade. The location of the street diagrams are shown on the Downtown Districts Map on page 4. Photos on each page illustrate key ideas.

Sections B-E contain guidelines that apply to all districts within the Downtown, related to the following topics:

- Building Design;
- Parking;
- Signs; and
- Street Improvements.

Sections F includes a glossary of terms.

RELATIONSHIP TO ZONING REGULATIONS

The regulations that govern Downtown development are established in the City's Development Ordinance. In the event of any discrepancies between the Development Ordinance and these Design Guidelines, the Development Ordinance governs as the legally binding document.

The Design Guidelines illustrate development standards in the Ordinance, and also show how more qualitative development concepts can be met.

DOWNTOWN DISTRICTS

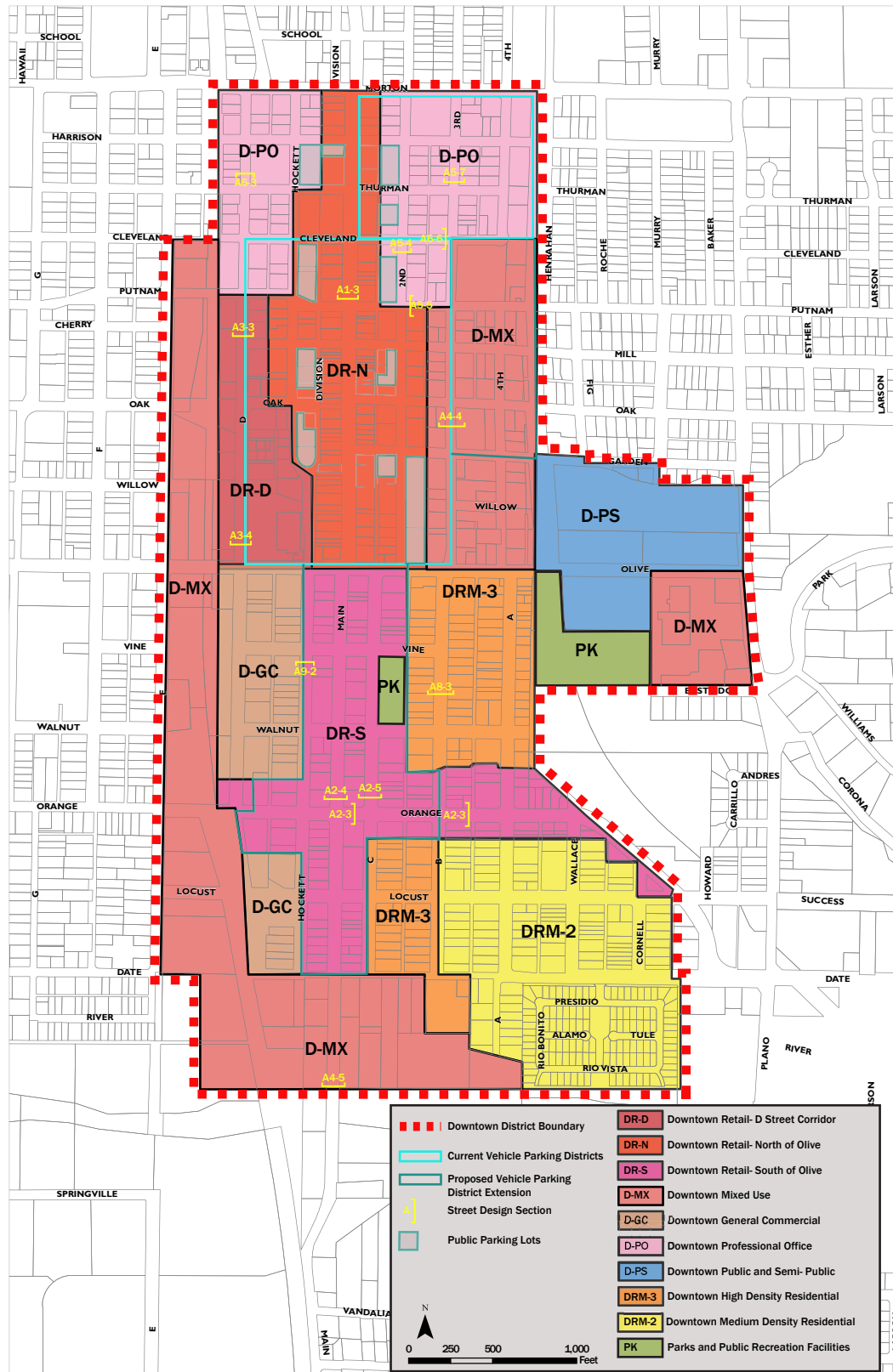
Downtown Porterville is a traditional downtown with a "Main Street" at the center, and a grid system of streets. Block sizes are small, so people can easily walk throughout the Downtown. Many blocks have alleys, which enables parking and loading to happen at the rear of the properties, and the front of the property to be a fully landscaped front yard without driveways. The natural edges of Downtown are Morton Street to the north, Henrahan and Plano Streets on the east, the river on the south, and the railroad right-of-way on the west.

Within the Downtown, there are different districts that have a distinctive character, based on:

- **Land uses** (such as retail, residential, and small office);
- **Streets** (wider streets that carry more traffic vs. narrower streets with less traffic); and
- **Building form** (such as retail buildings built right at the sidewalk edge, small homes converted to offices, or shopping centers with parking lots in front of the buildings).

The following map shows the nine Downtown districts. Each district has a unique set of development standards and guidelines based on the existing character of the district and the City's goals for future improvements, as described in Section A.

Downtown Districts Map



A. BUILDING FORM AND BUILDING TO STREET RELATIONSHIP

1. DOWNTOWN RETAIL – NORTH OF OLIVE AVENUE (DR-N)

Purpose:

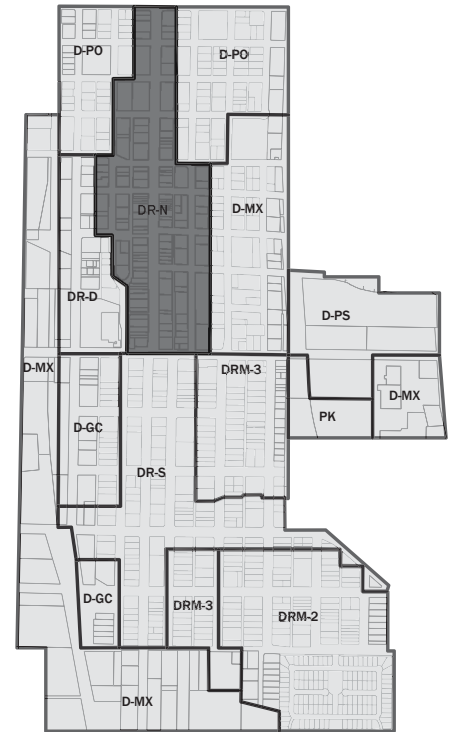
- Maintain the pedestrian- and transit-oriented environment of retail, restaurants, services, and government offices in the heart of Porterville’s Downtown, focused on Main Street. Maintain the traditional Downtown character, with buildings built immediately adjacent to the sidewalk, active commercial uses on the ground floor, ample on-street parking, and a visually attractive streetscape, with trees, historic street lights, benches, and other pedestrian amenities.

Character:

- Maintain the traditional pedestrian-oriented “Main Street” character, with shops and restaurants lining Main Street, and residential or office uses on upper floors.
- Renovate the façades of existing and historic buildings, consistent with the original architecture, to retain the traditional character of Downtown.
- Design new buildings consistent with the existing development pattern in Downtown, and in a style which is compatible with the traditional and historic character of Main Street.

Development Density and Building Heights:

- Maximum Floor Area Ratio: 3
- Maximum Dwelling Units per Acre: n/a
- Maximum Building Height:
 - 50 feet (3-4 stories) on corner lots and lots over 2 acres
 - 40 feet (1-3 stories) on all other lots
- Minimum Building Height on Main Street: 28 feet

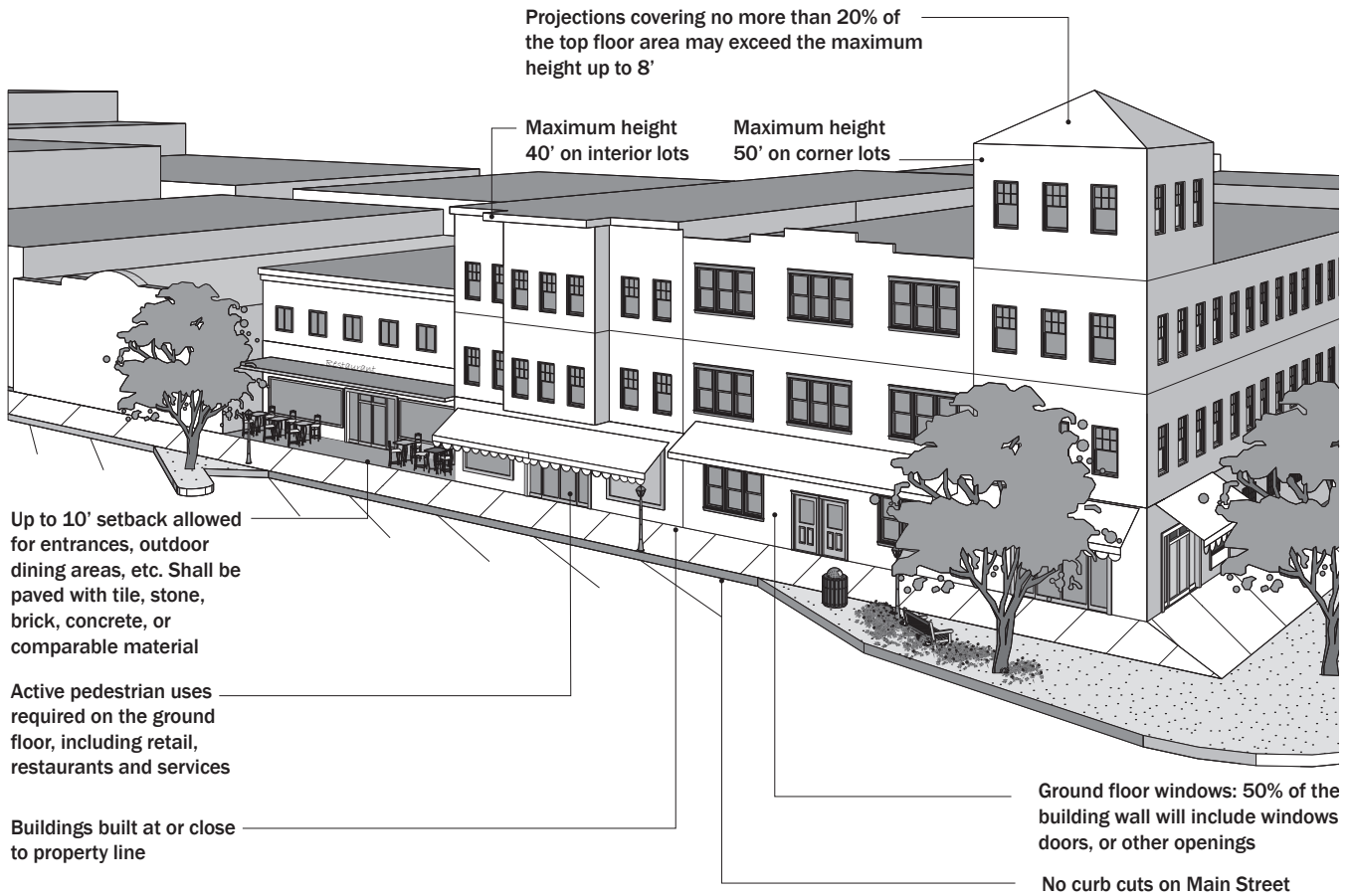


Buildings on Main Street line the street with angled parking and regular landscaping.



Streets off of Main Street are well landscaped. Parking is located on side streets.

Figure A1-1:
Downtown Retail North: Building Form



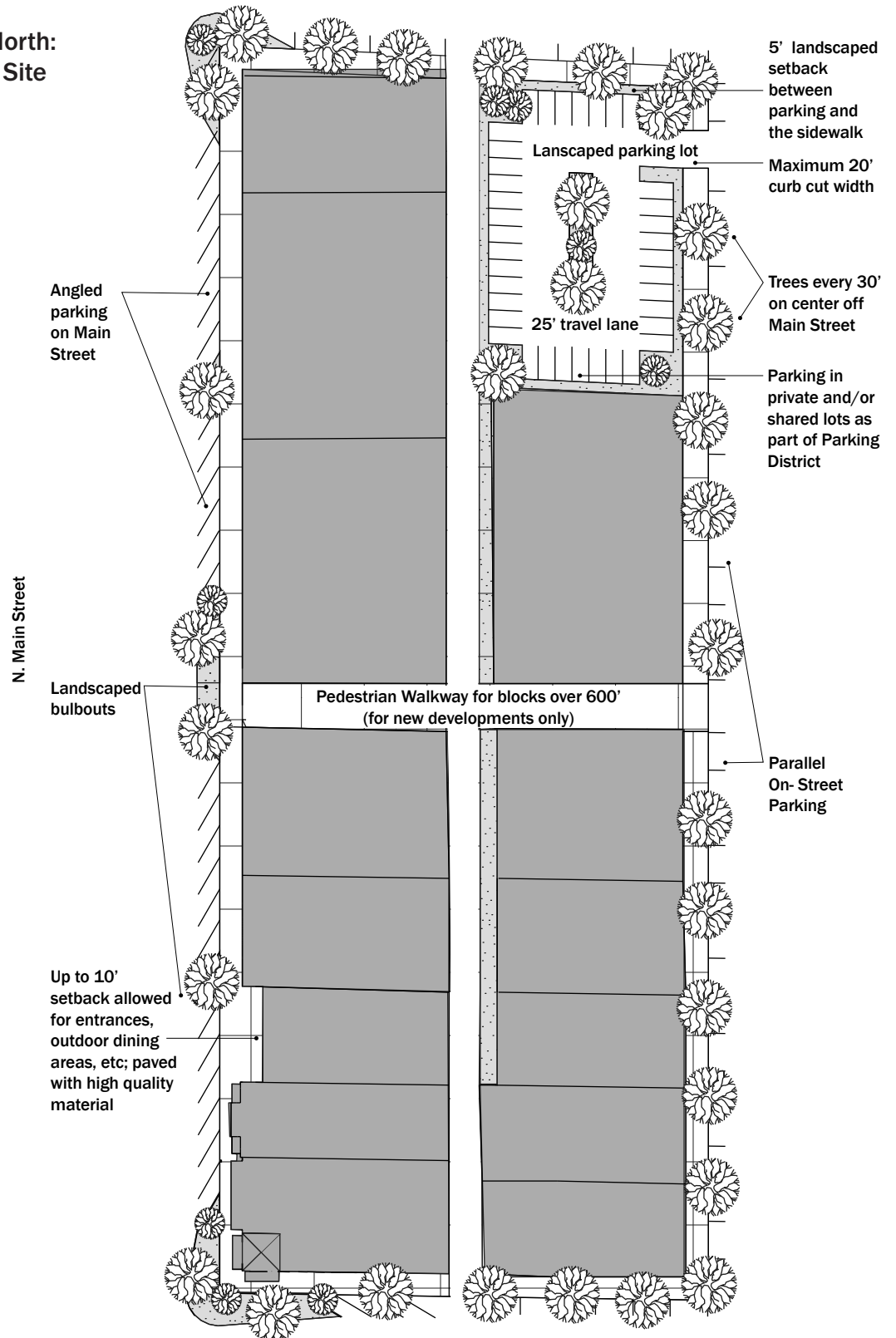
The corner of Olive Avenue and Main Street is the entry to the heart of the DR-N District.



New development located in DR-N maintains the character of Downtown Porterville.

DOWNTOWN RETAIL – NORTH OF OLIVE AVENUE

Figure A1-2:
Downtown Retail North:
Building Form and Site
Planning





Street trees that provide shade and still ensure the visibility of business signs.



Sidewalk shaded by overhangs, awnings, and street trees.



Trees and landscaping buffer the sidewalks from surface parking lots.

Building Form – Key Features:

- Locate buildings to line the street, built immediately adjacent to the sidewalk, in a continuous row without parking lots or side yards between buildings.
- Locate active commercial uses on the ground floor – stores, restaurants, personal services.
- Provide angled and parallel parking on streets in front of businesses.
- Locate public parking lots behind the row of buildings on Main Street.
- Locate service and delivery areas at the rear of buildings.
- Shade sidewalks with awnings and building projections.
- Place outdoor café areas along the sidewalk edge, or in sidewalks if there is room for a pedestrian walkway and ample handicapped access.

Street Design on Main Street – Key Features:

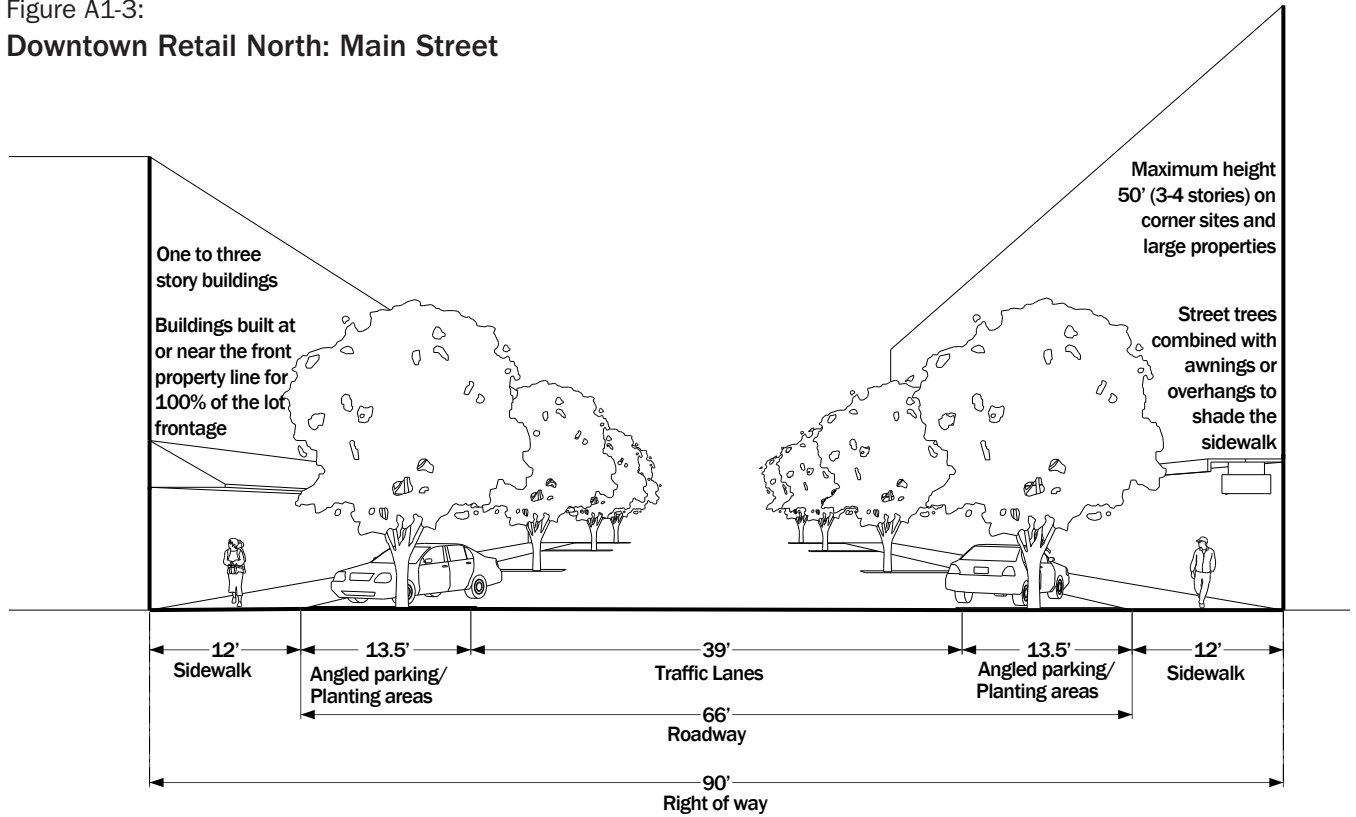
- Maintain historic street lights and special paving materials to identify Main Street as the central shopping area.
- Provide benches, trash receptacles, landscaping, and other pedestrian amenities.
- Provide corner bulb-outs to ensure safe and attractive pedestrian street crossings.
- Locate street trees in islands and bulb-outs, widely spaced to allow the visibility of Downtown businesses.
- Do not locate curb cuts or driveways on Main Street.

Street Design on Other Streets – Key Features:

- Provide a continuous rows of street trees, located in sidewalks or planter strips.
- Shade sidewalks with awnings and building projections.
- Locate buildings to line the street, 0-10 feet from the sidewalk edge.

DOWNTOWN RETAIL – NORTH OF OLIVE AVENUE

Figure A1-3:
Downtown Retail North: Main Street



All new construction and major renovation projects shall install and/or maintain streetscape improvements in the public right-of-way, including: sidewalks, street trees, and awnings or overhangs to shade the sidewalk. For street improvements on side streets (i.e. other than Main Street), see Section E: Street Improvements.



Main Street is a pedestrian-oriented street with slow traffic speed and bulb-outs at intersections to create safe, comfortable pedestrian crossings.



The Downtown streetscape design creates a distinctive historic character, with historic street lights, street trees, landscaped islands, benches, and trash receptacles.

Figure A1-4:

Downtown Retail North: New Construction Guidelines for Period Architecture

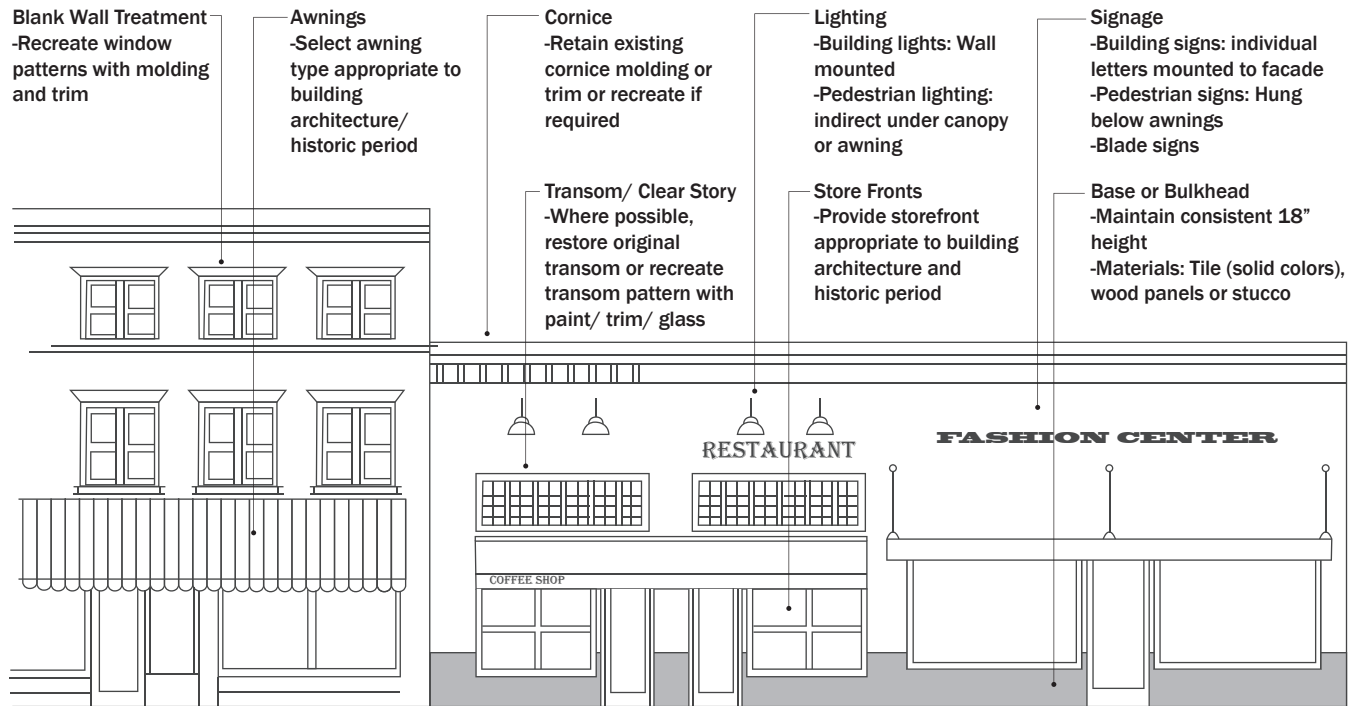


Figure A1-5:
Downtown Retail North: Façade Renovation Guidelines

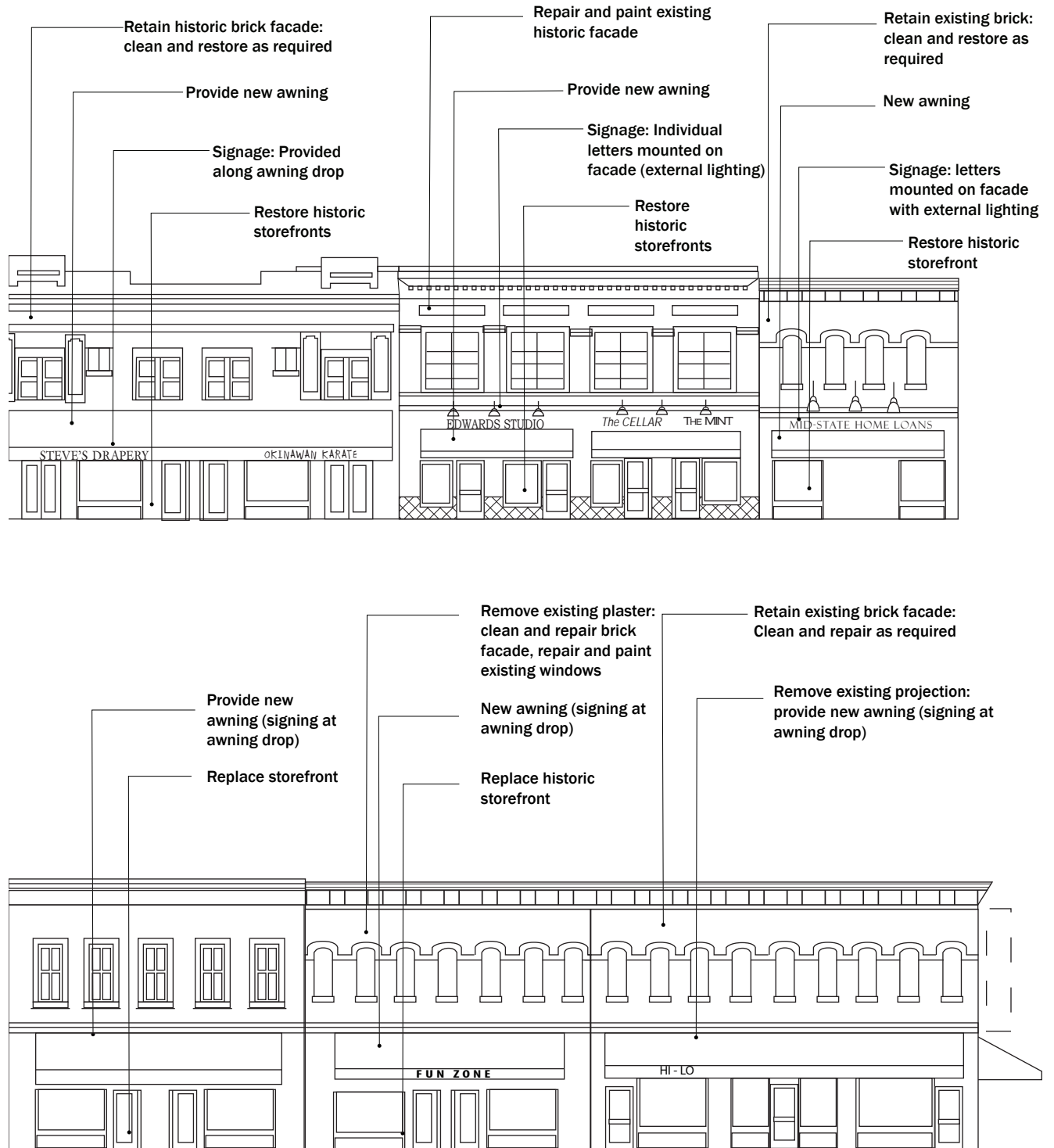
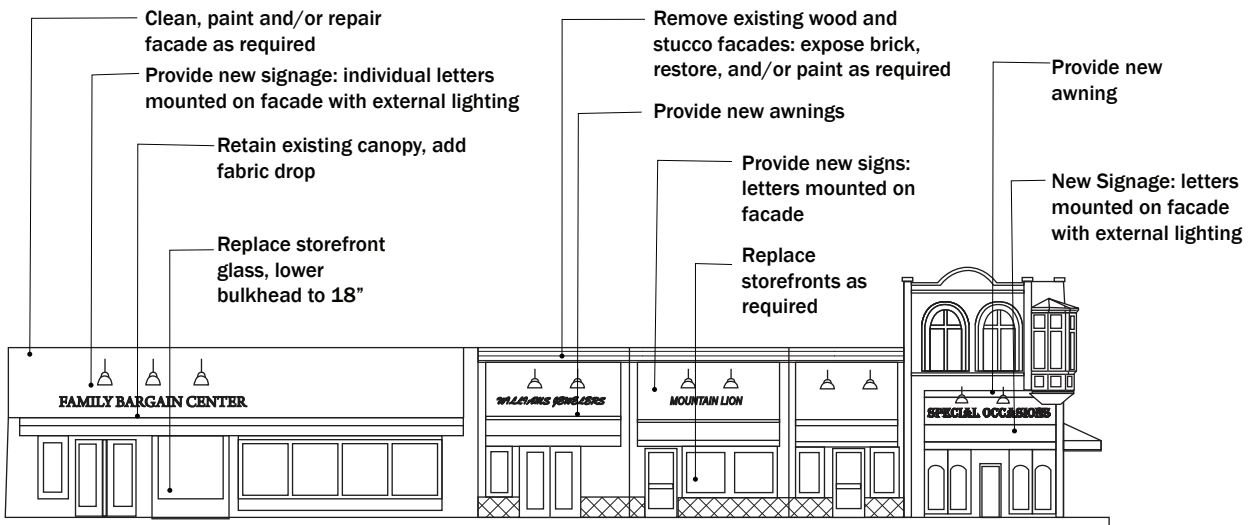
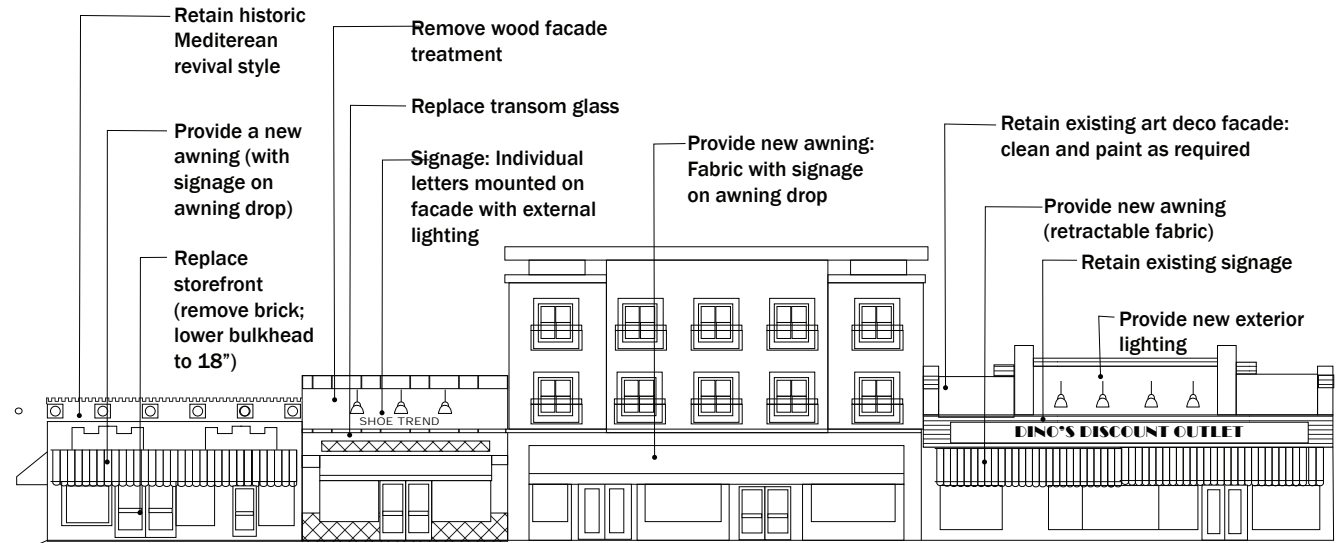


Figure A1-6:
Downtown Retail North: Façade Renovation Guidelines



DOWNTOWN RETAIL – SOUTH OF OLIVE AVENUE

2. DOWNTOWN RETAIL – SOUTH OF OLIVE AVENUE (DR-S)

Purpose:

Create an attractive commercial corridor that provides a welcoming gateway at the southern entrance to Downtown. Buildings should line the street, with street trees along the entire length of the street, and small landscaped front yards between the buildings and the sidewalk.

Character:

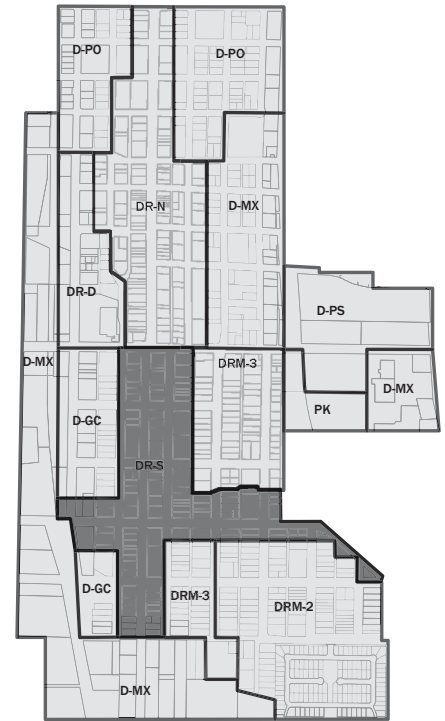
- Promote a pedestrian oriented area with buildings that line the street, street trees along the entire length of the street, and small landscaped front yards between the buildings and the sidewalk.
- Establish a continuous linear pattern of street trees and landscaping to create an attractive gateway.

Development Density and Building Heights:

- Maximum Floor Area Ratio: 3
- Maximum Dwelling Units per Acre: n/a
- Minimum Percent of Building at Setback Line: 50 percent
- Maximum Building Height:
 - 50 feet (3-4 stories) on corner lots and lots over 2 acres
 - 40 feet (1-3 stories) on all other lots

Building Form – Key Features:

- Locate buildings to line the street, with 10 foot landscaped front yards between the sidewalk and the building.
- Locate parking on the streets in front of businesses, and to the side and rear of buildings.
- Include landscaped areas at least 10 feet deep to buffer sidewalks from surface parking areas on Main Street. Include 5 feet buffers on other streets. These landscape buffers include street trees.
- Shade sidewalks with awnings and building projections.



Buildings are generally 1-2 stories. In many places awning and street trees are required to shade the sidewalk.



Buildings line with street with some buildings setback 10-15 feet. Streets are wide with on-street parking available.

Figure A2-1:
Downtown Retail South: Building Form

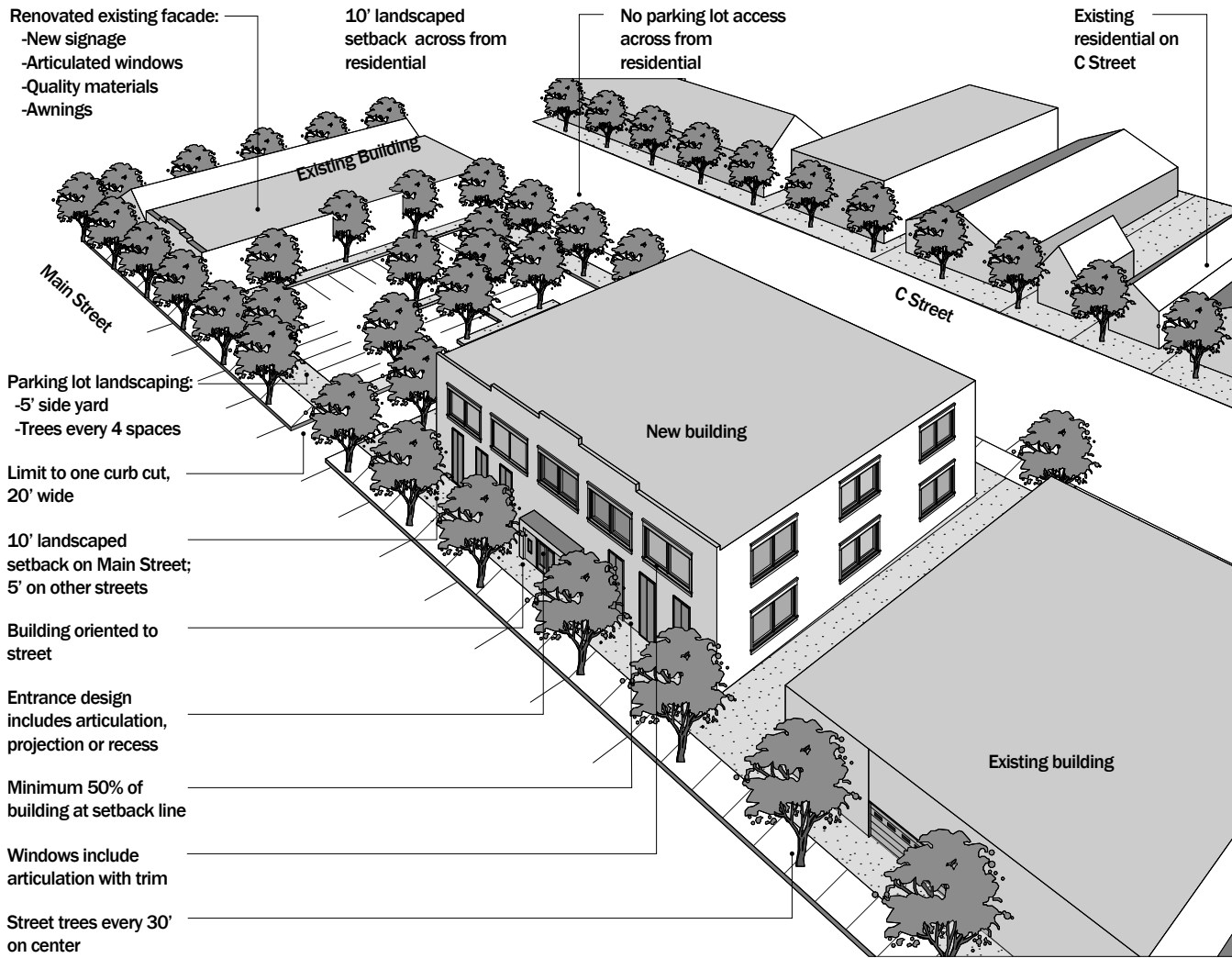
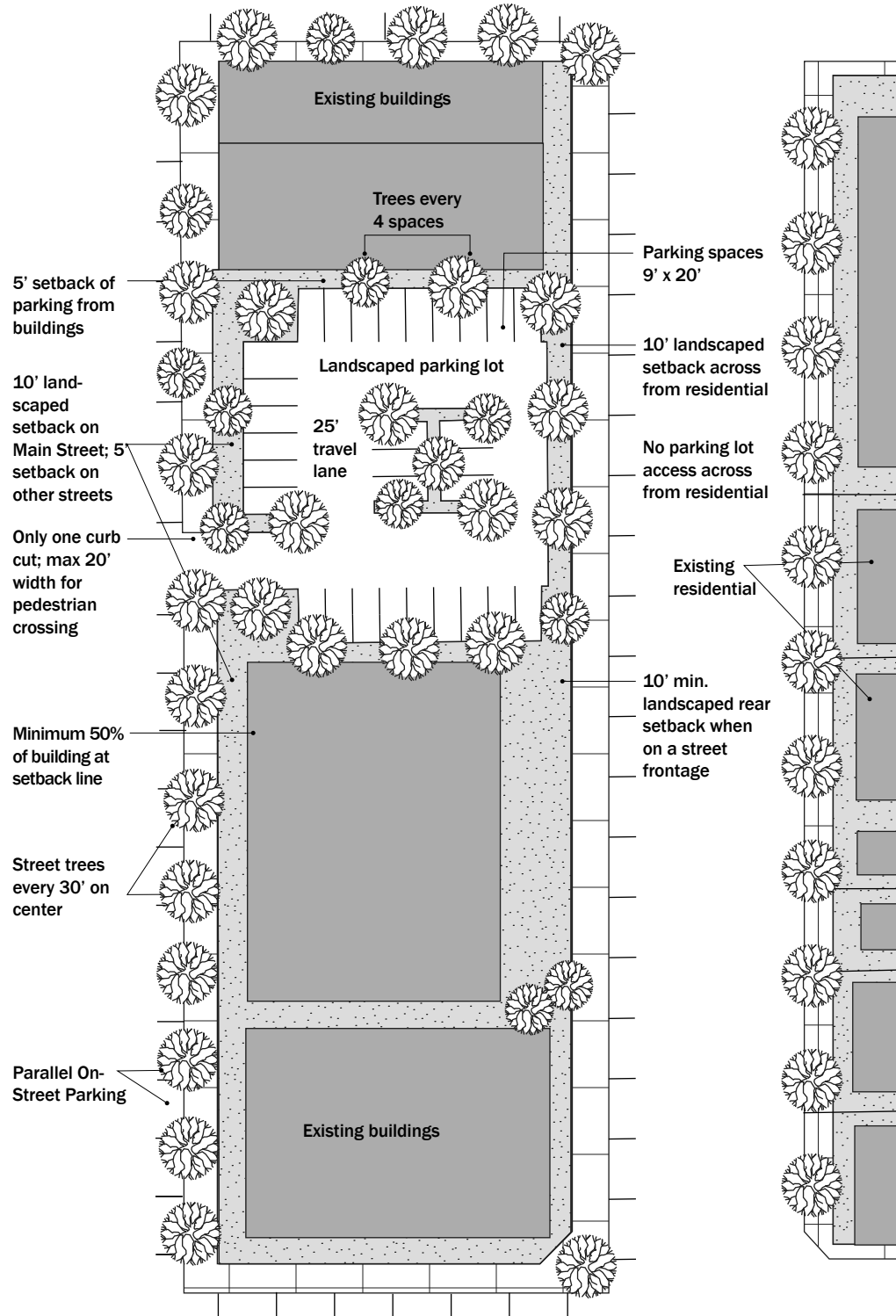


Figure A2-2:
Downtown Retail South: Building Form and Site Planning





Preserve the landscaped median on Orange Street. Additional street trees are needed to shade the sidewalk.

Street Design on Orange Street – Key Features:

- Maintain Orange Street as a major east-west through street with two traffic lanes in each direction.
- Preserve the existing landscaped median, and extend it west from Main Street to D Street.
- Add a continuous row of street trees, located in sidewalks or planter strips that shade the sidewalk and create an attractive street character.
- Create a double row of trees, by planting trees in front yards, in addition to trees along sidewalk, to shade the sidewalks.



Awnings and street trees would improve the pedestrian friendliness of S. Main Street.

Street Design on Main Street – Key Features:

- Add a landscaped median approximately 12 feet wide in the middle of the street, allowing for turn lanes at intersections.
- Install a continuous row of street trees in the sidewalk or in the parking lane in the street, between parking spaces.
- Install historic street lights.
- Add corner bulb-outs to make safe and attractive pedestrian street crossings.
- Minimize curb cuts or driveways along Main Street.



Recent improvements on C Street included a continuous row of street trees and historic lighting.

Street Design on Other Streets (i.e. C Street) - Key Features

- Add a continuous row of street trees, located in sidewalks or planter strips that shade the sidewalk and create an attractive street character.
- Create a double row of trees, by planting trees in front yards, in addition to trees along sidewalk, to shade the sidewalks.

DOWNTOWN RETAIL – SOUTH OF OLIVE AVENUE

Figure A2-3:
Downtown Retail South: Orange Street Enhancements

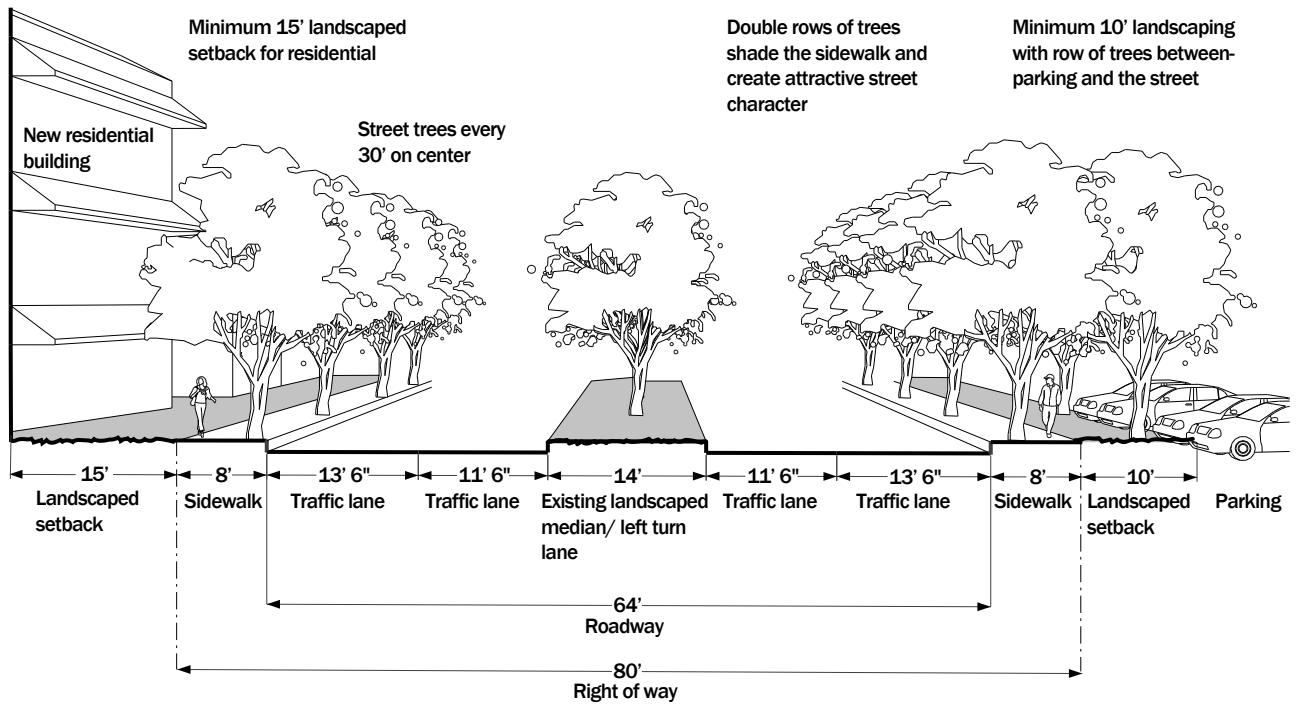
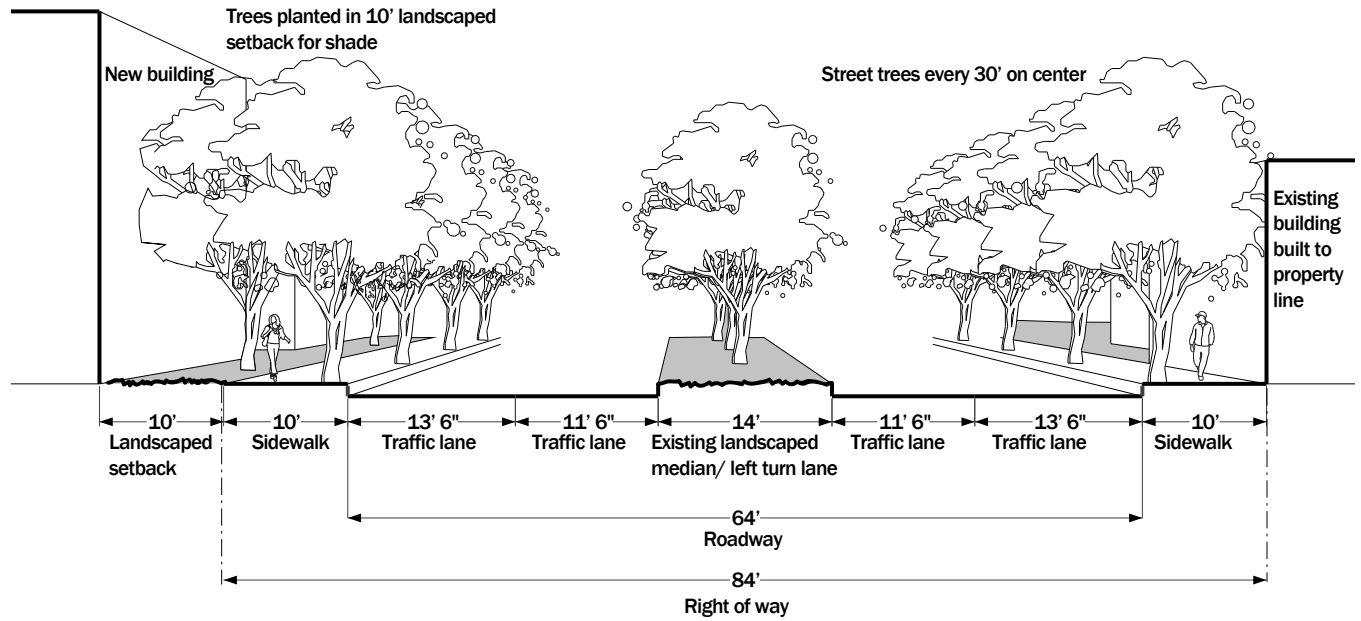


Figure A2-4:
Downtown Retail South: Main Street Enhancements

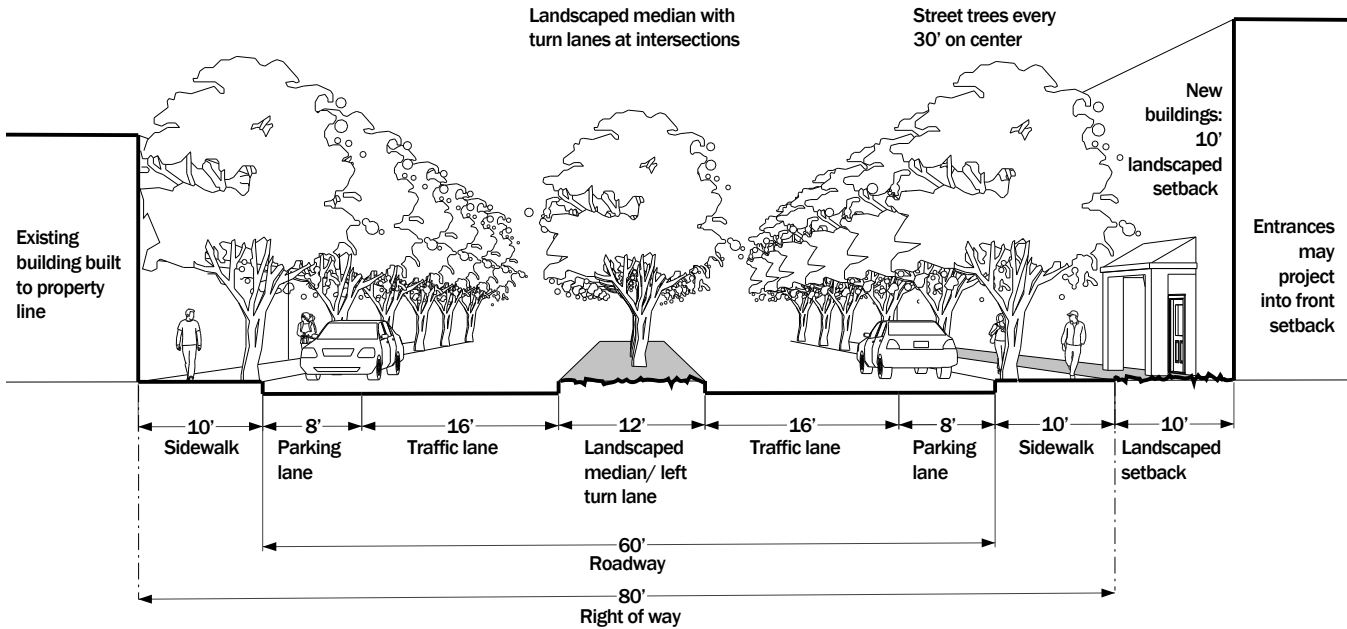
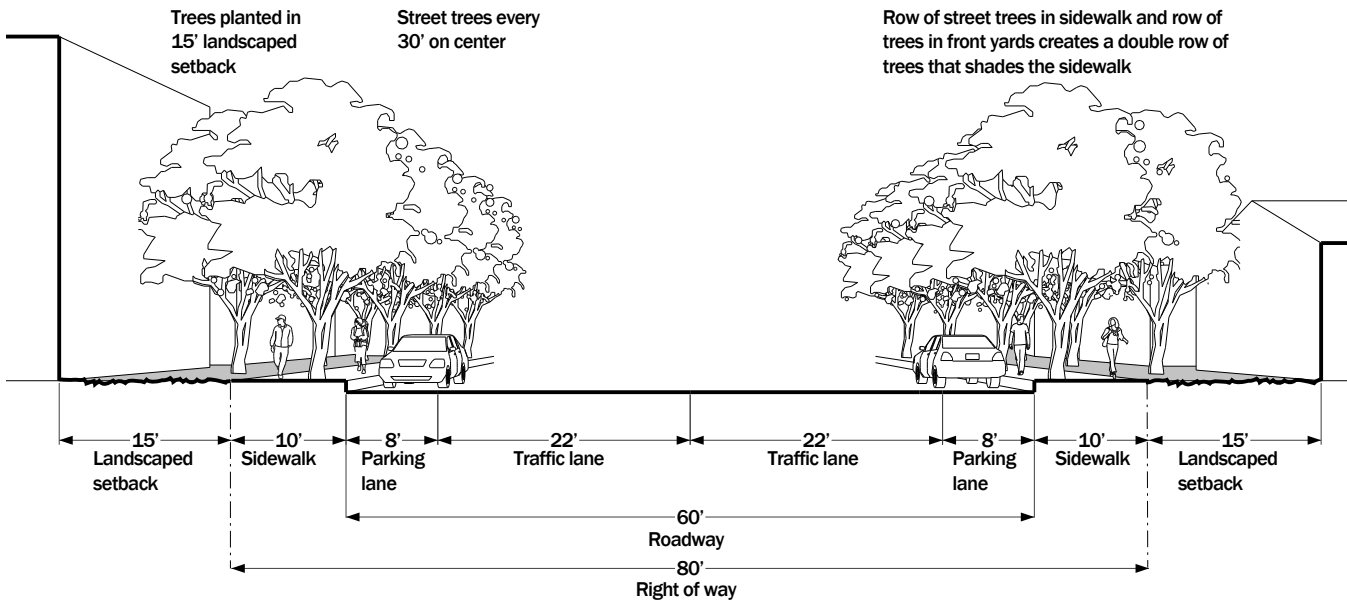


Figure A2-5:
Downtown Retail South: Other Street Enhancements (C Street)



DOWNTOWN RETAIL – D STREET CORRIDOR

3. DOWNTOWN RETAIL – D STREET CORRIDOR (DR-D)

Purpose:

Provide for a mix of medium-scale retail and service uses in a manner compatible with the Downtown pedestrian environment.

Character:

Promote a pedestrian oriented retail and services district with buildings that are oriented to the street, with deep landscaped front yards and rows of trees.

Development Density and Building Heights:

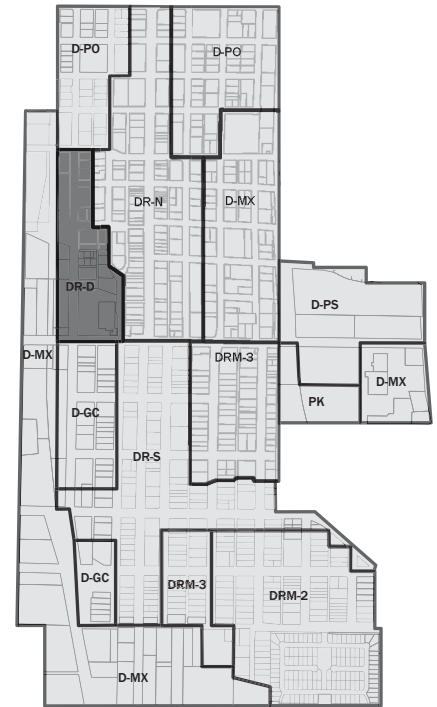
- Maximum Floor Area Ratio: 3
- Maximum Dwelling Units per Acre: n/a
- Minimum Percent of Building at Setback Line: 50 percent
- Maximum Building Height:
 - 50 feet (3-4 stories) on corner lots and lots over 2 acres
 - 40 feet (1-3 stories) on all other lots

Building Form – Key Features:

- Locate buildings to line the street, with a 10-20 foot landscaped front yard between the sidewalk and the building.
- Locate parking on the street in front of businesses and in lots to the side and rear of buildings.
- Include landscaped areas of 5 to 10 feet between sidewalks and surface parking areas on D Street. These landscape buffers include street trees.

Street Design on D Street – Key Features:

- Add a landscaped median approximately 17 feet wide in the middle of the street, allowing for turn lanes at intersections.
- Install a continuous row of street trees in the sidewalk (or in the planter strips where they exist).
- Create a double row of trees by planting trees in front yards to shade the sidewalks where feasible.
- Add corner bulb-outs to make safe and attractive pedestrian street crossings.

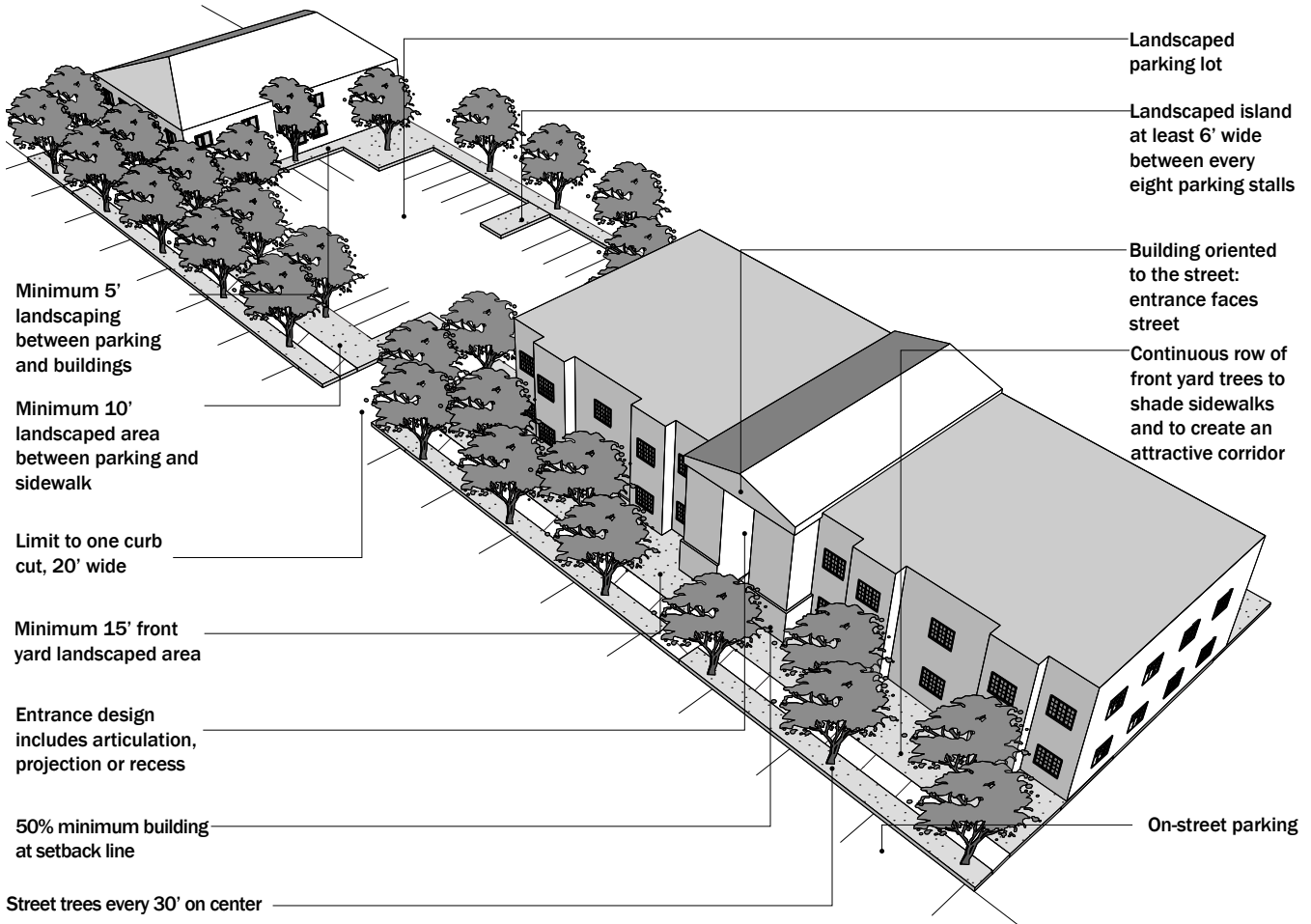


Street trees are needed on D Street to shade the sidewalk.



Existing width of D Street could be improved with a landscaped median strip between Putnam Ave and Olive Ave.

Figure A3-1:
Downtown Retail D Street: Building Form

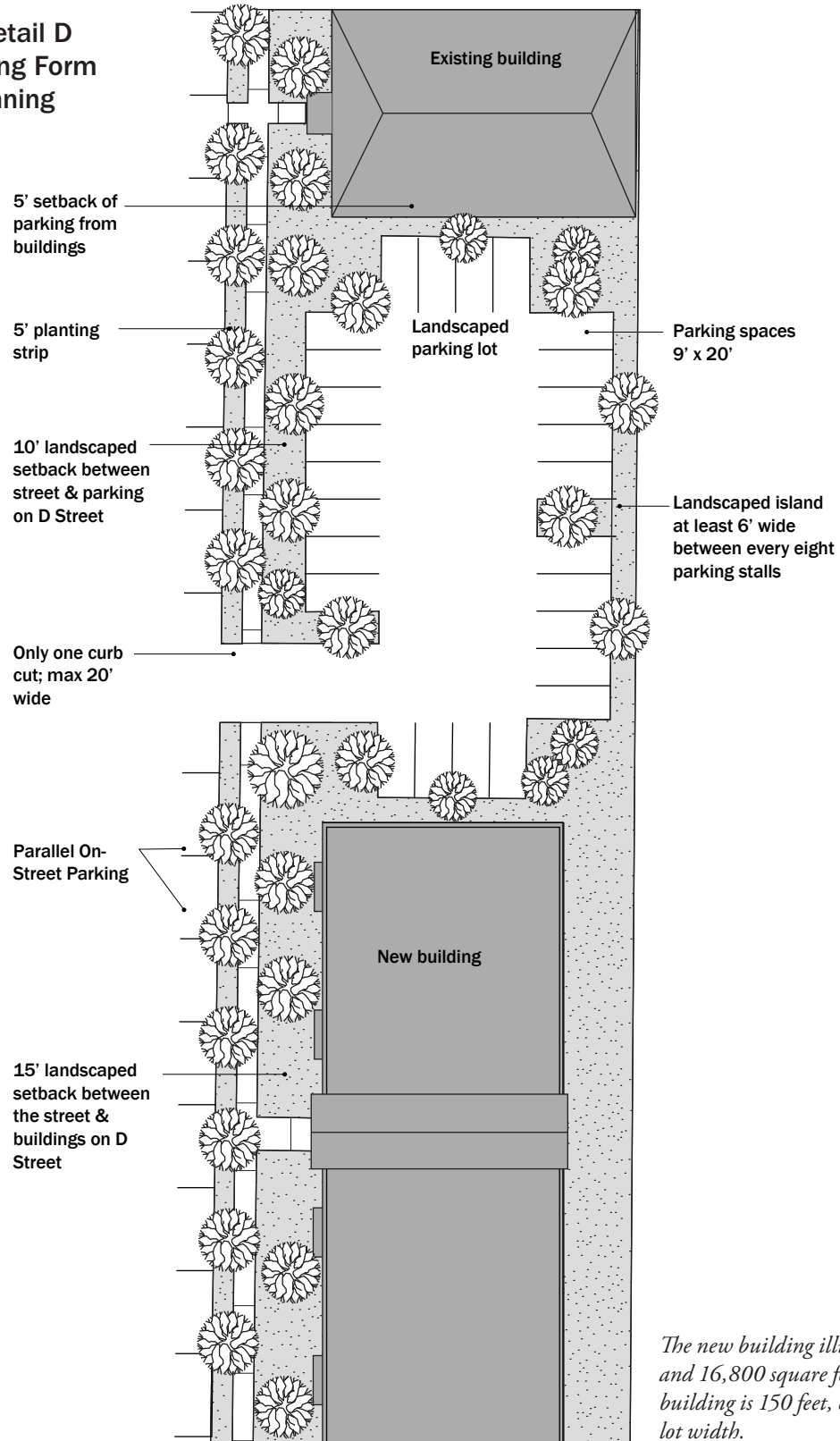


Landscaped setbacks and landscaped parking lot create an attractive image on D Street.



On D Street, a 10 foot planting strip with trees separates parking from the sidewalk.

Figure A3-2:
Downtown Retail D
Street: Building Form
and Site Planning



The new building illustrated is two stories and 16,800 square feet. The front of the building is 150 feet, over 50 percent of the lot width.

Figure A3-3:
Upper D Street Enhancements

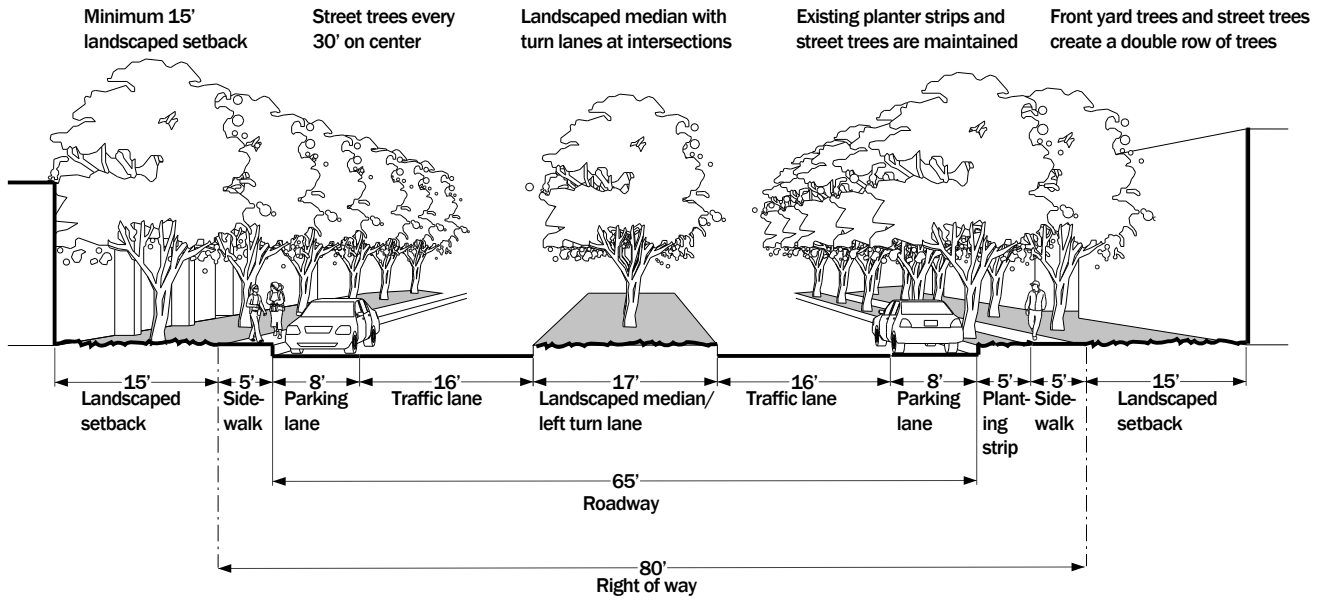
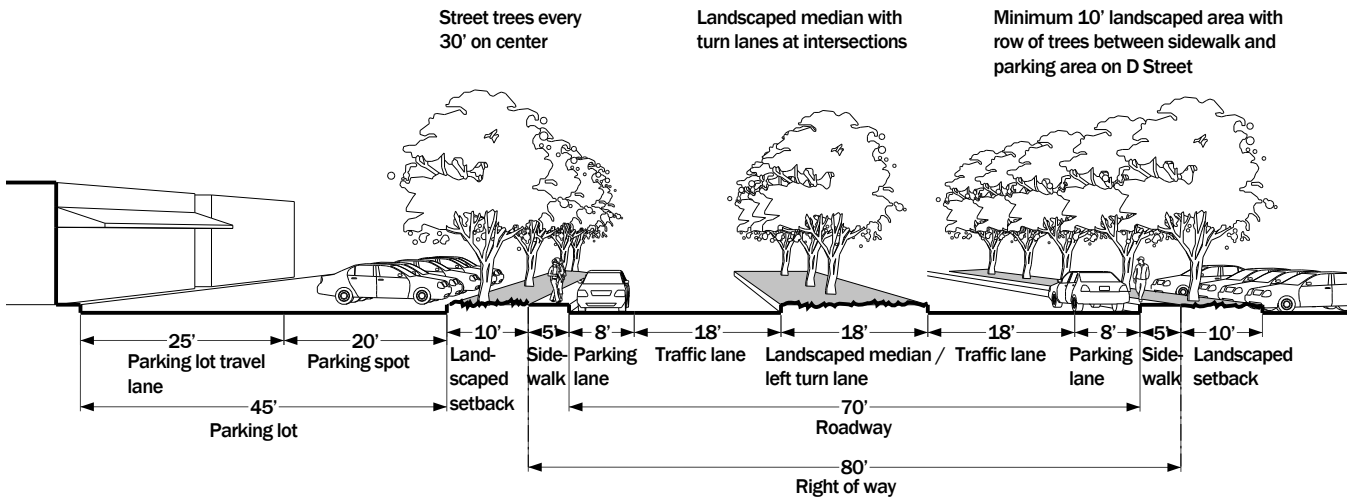


Figure A3-4:
D Street Shopping Centers



Note: The 50 percent building at setback line requirement may be waived with conditional use permit approval for retail stores or shopping centers with a gross floor area of 50,000 square feet or greater when the City Council finds that alternative features have been incorporated into the project to achieve pedestrian orientation and to accommodate pedestrians and bicycle access.

4. DOWNTOWN MIXED USE (D-MX)

Purpose:

Create a mixed-use area that attracts businesses and residents to locate Downtown, and provides a transition area between commercial areas and residential neighborhoods. Allow a mix of commercial, service, office, and residential uses that do not adversely impact one another in a significant way. Create an attractive streetscape of buildings that line the street, with landscaping and trees tailored to the width and the existing design character of the street.

Character:

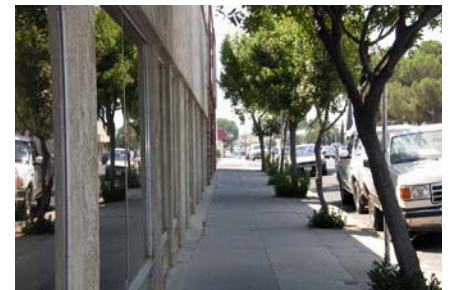
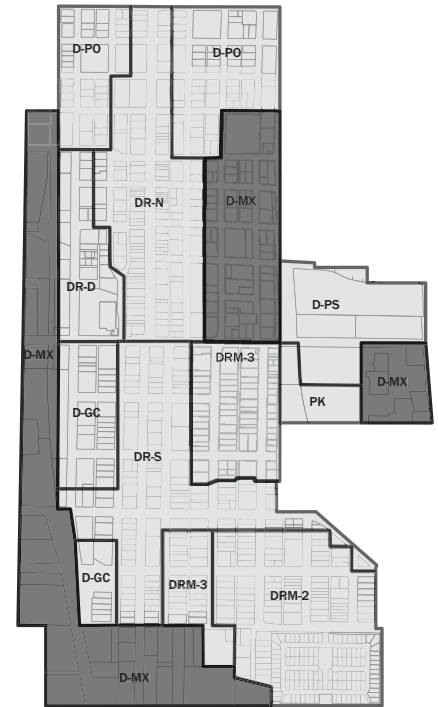
- Provide a mix of uses and typologies that create a diverse streetscape with varied setbacks and building heights.
- Line streets with trees and additional landscaping in front yard setbacks for residential units.
- Orient buildings to the street and locate commercial uses at the street edge.
- The Main Street D-MX zone creates an entry way into Downtown Porterville.

Development Density and Building Heights:

- Maximum Floor Area Ratio: 3
- Minimum Percent of Building at Setback Line: 50 percent
- Minimum Dwelling Units per Acre: 15
 - Applicable only to new construction of residential only projects
- Maximum Dwelling Units per Acre: 41.0
- Maximum Building Height:
 - 40 feet (1-3 stories)
 - 50 feet with Conditional Use Permit

Building Form – Key Features:

- Vary building form by use, with residential uses set back 10 feet from the street and commercial buildings lining the street, set back 0-10 feet.
- Locate residential parking off of alley ways or side streets where feasible. Shared parking access to minimize curb cuts is preferred.
- Locate commercial parking on the street in front of businesses, and in lots to the side and rear of buildings.



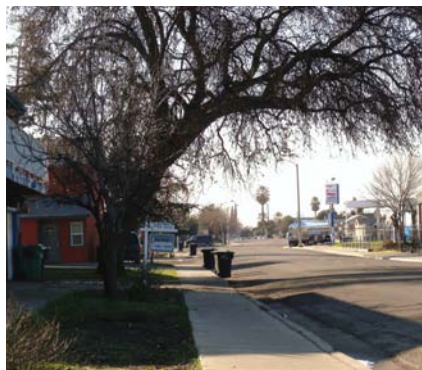
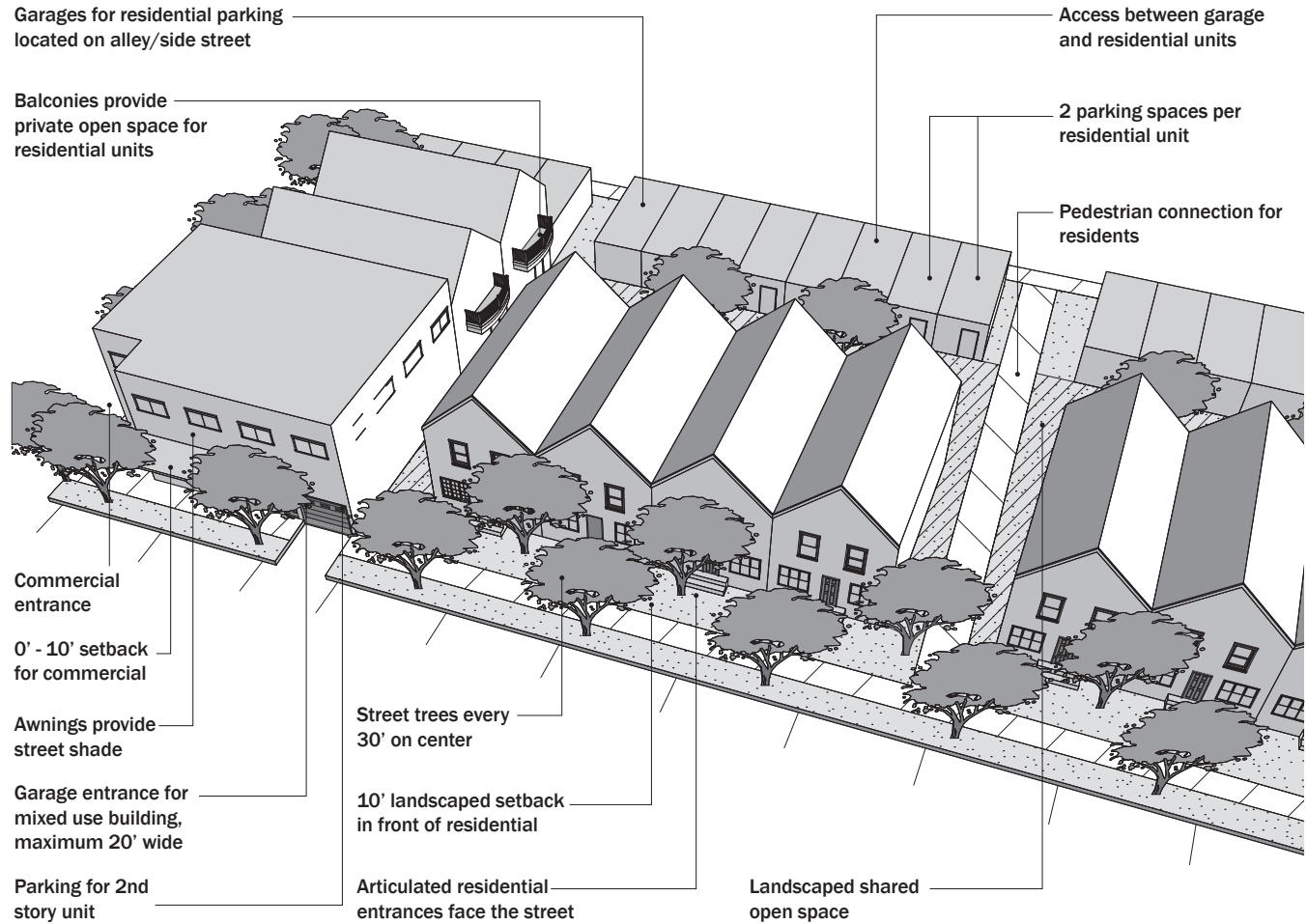
Commercial buildings are located at the street edge and streets are lined with trees.



The building typology is mixed, with commercial uses at the street edge and residential uses set back. Residential setbacks are landscaped.

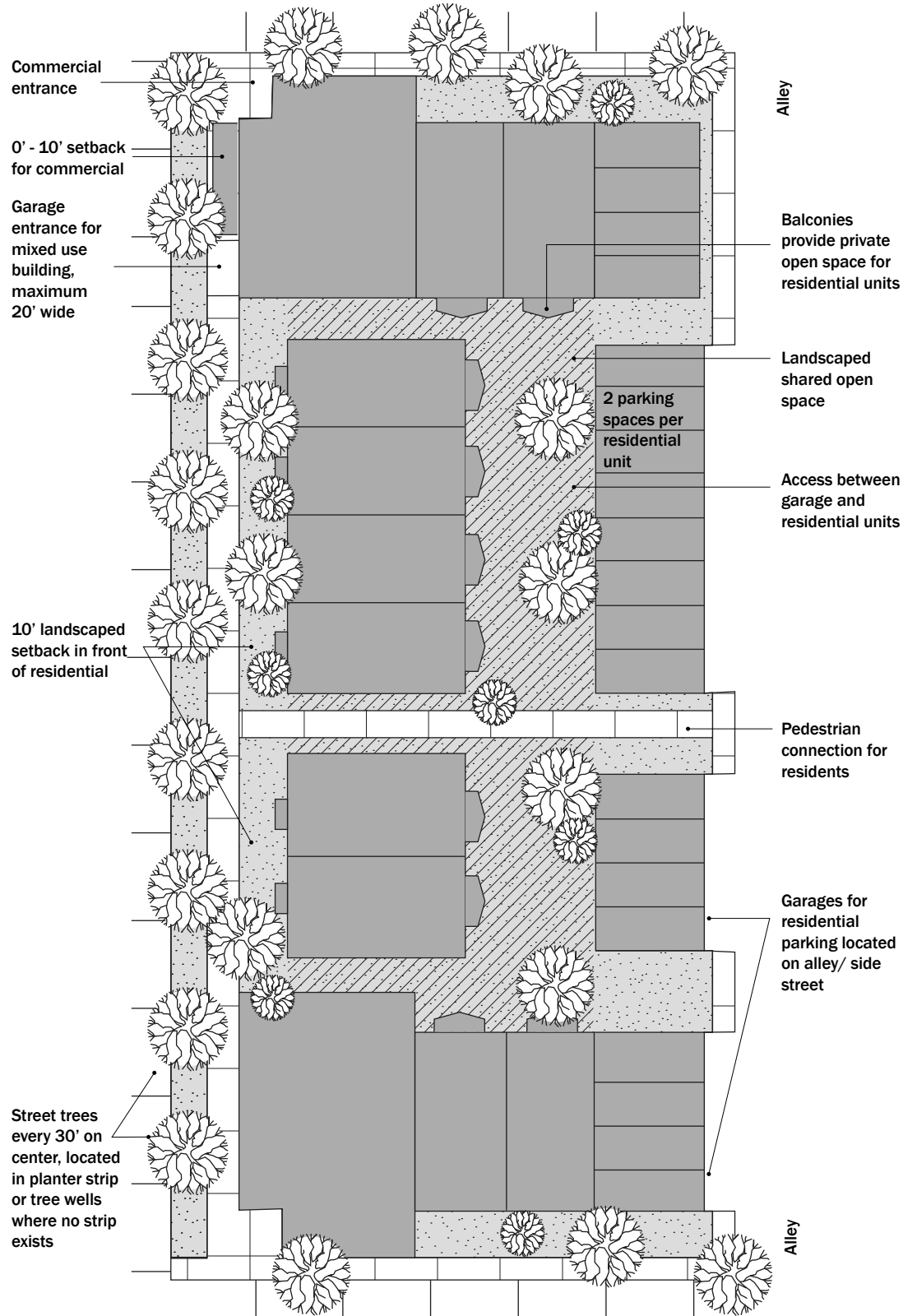
Figure A4-1:
**Downtown Mixed Use:
 Building Form**

The mixed use block depicted here and in the site plan on the following page has an overall density of 16.7 units per acre. The block is made up of several 1,600 square feet 2 story townhouses and a corner building with 2,000 square feet of commercial space on the ground floor and a residential unit on the second floor.



Existing buildings in the D-MX areas are varied, generally ranging from 1 to 3 stories. New development should include well articulated residential development and landscaping that creates a pedestrian friendly environment consistent with Downtown.

Figure A4-2:
**Downtown
 Mixed Use:
 Building Form
 and Site
 Planning**





Sites off of the southern end of Main Street offer opportunities for large scale or campus style development.



Existing markers of the gateway into the Downtown District on Main Street.

Large Site Building Form– Key Features:

There are several large sites located off of Main Street at the southern end of the Downtown district and along the railroad corridor. The large sites along Main Street bridge the area between the river and the Downtown Retail - South of Olive area. These sites range in size from around an acre to nearly five acres. If redeveloped, these sites could be used for large scale or campus style development. The following guidelines apply to those larger sites located in the D-MX zone.

- Provide a unified site plan for developments on large sites, generally applying to sites over one acre.
- Locate buildings to line primary streets, especially Main Street.
- Locate commercial parking on the street in front of businesses, and at the side and rear of buildings.
- Ensure pedestrian access and pedestrian connections throughout the project.

Street Design – Key Features:

- Install a continuous row of street trees in the sidewalk or in the planter strips where they exist.
- Create a double row of trees by planting trees in front yards (where possible) in addition to trees along the sidewalk, to shade the sidewalk.
- Shade streets with awnings and building projections where buildings sit at the street edge.
- Provide a minimum 10 foot landscaped setback on Main Street.

Figure A4-3:
Downtown Mixed Use Large Site Building Form and Site Planning

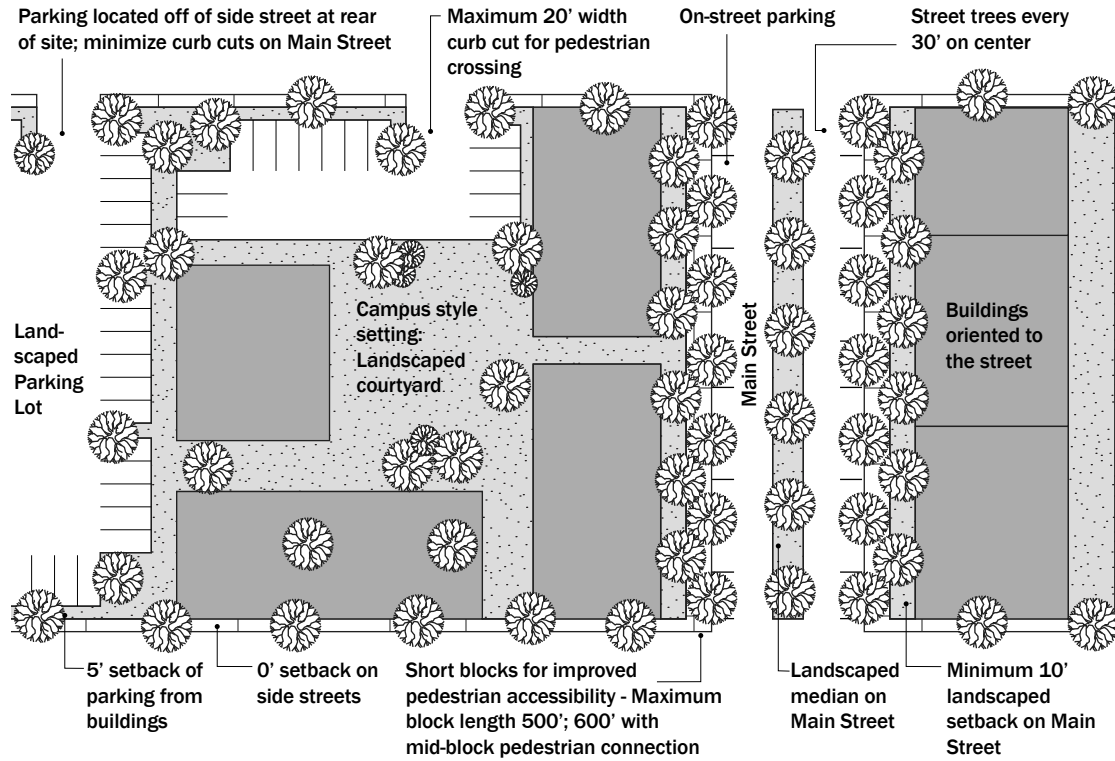


Figure A4-4:
Downtown Mixed Use: 3rd and 4th Street Enhancements

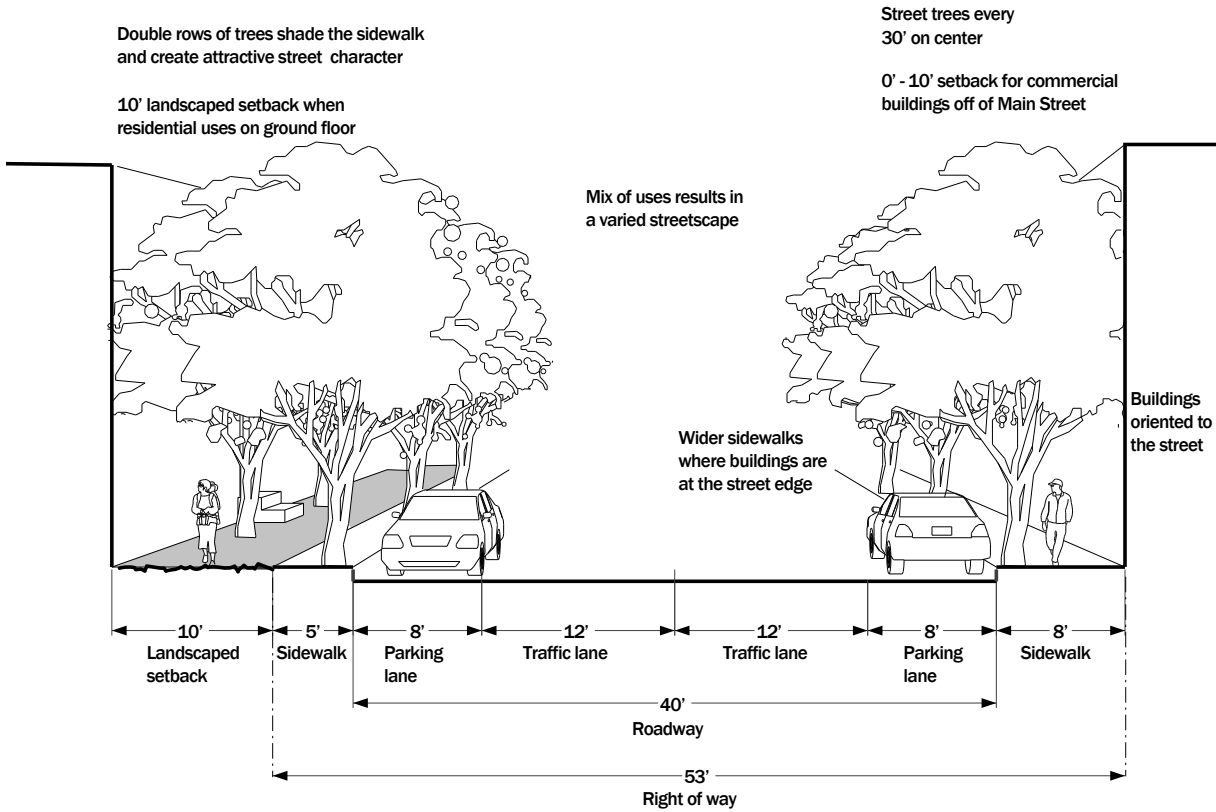
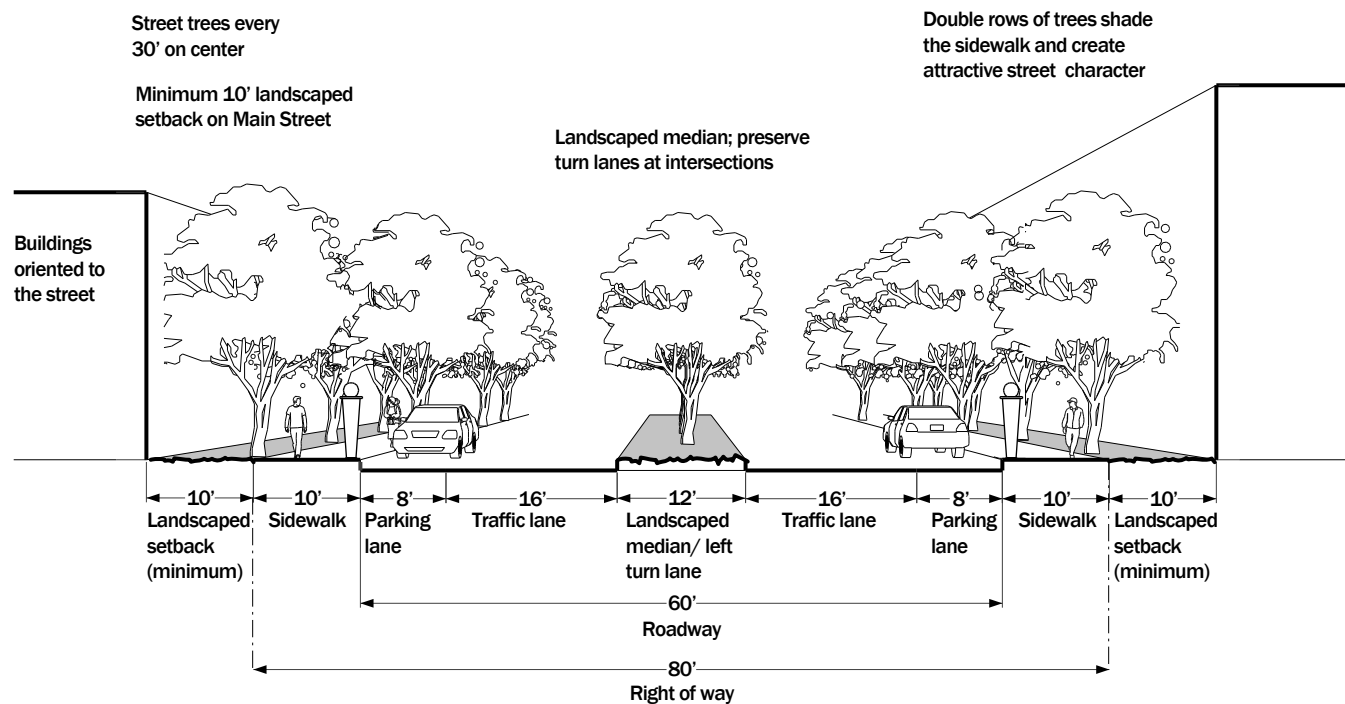


Figure A4-5:
Downtown Mixed Use: Main Street Enhancements



DOWNTOWN PROFESSIONAL OFFICE

5. DOWNTOWN PROFESSIONAL OFFICE (D-PO)

Purpose:

Encourage the adaptive re-use of existing buildings with office and professional uses, and infill of new small office buildings, in order to provide convenient services to local residents and businesses, complement nearby retail development, and maintain the historic residential character of the area.

Character:

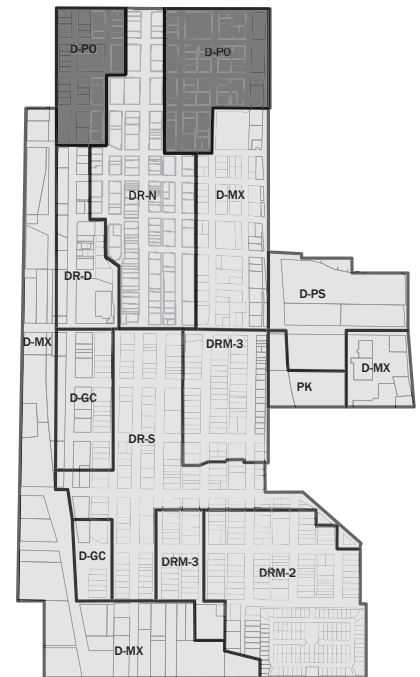
Maintain the traditional pattern of small lots, alleys, historic residential building forms, landscaped front yards, and street trees in planter strips.

Development Density and Building Heights:

- Maximum Floor Area Ratio: 0.5
- Maximum Building Height:
 - 30 feet (1-2 stories)
 - 40 feet with a Conditional Use Permit

Building Form – Key Features:

- Maintain pattern of small buildings, due to the small size of the lots.
- Orient fronts of buildings and building entrances to the street, with a 15-25 foot landscaped front yard between the sidewalk and the building.
- Cover no more than half of the lot with buildings and provide landscaped front, side, and rear yards around the buildings.
- Locate parking on the street, in garages or small parking lots accessed from the alley, or in a narrow driveway located along the side of the property.
- Do not locate parking in the front yard.
- Maintain predominant building form: residential, with a front porch, pitched roof, and other typical features of homes built in the 1920's.



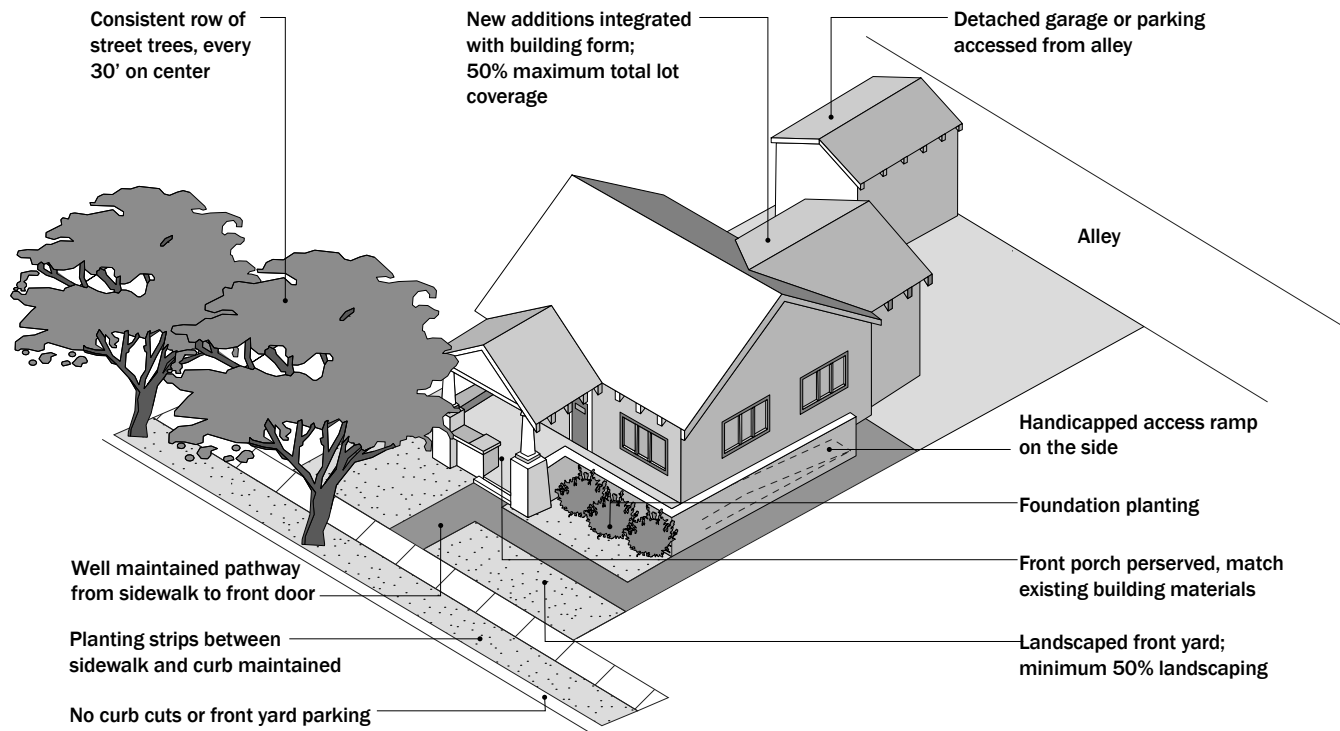
Typical buildings are residential homes converted to office, generally small buildings of 1-2 stories that face the street and have landscaped front yards.



Typical streets are two-lane local streets with parking on both sides, planter strips, and a consistent row of street trees.

Figure A5-1:

Downtown Professional Office: Building Form - Renovations and Additions

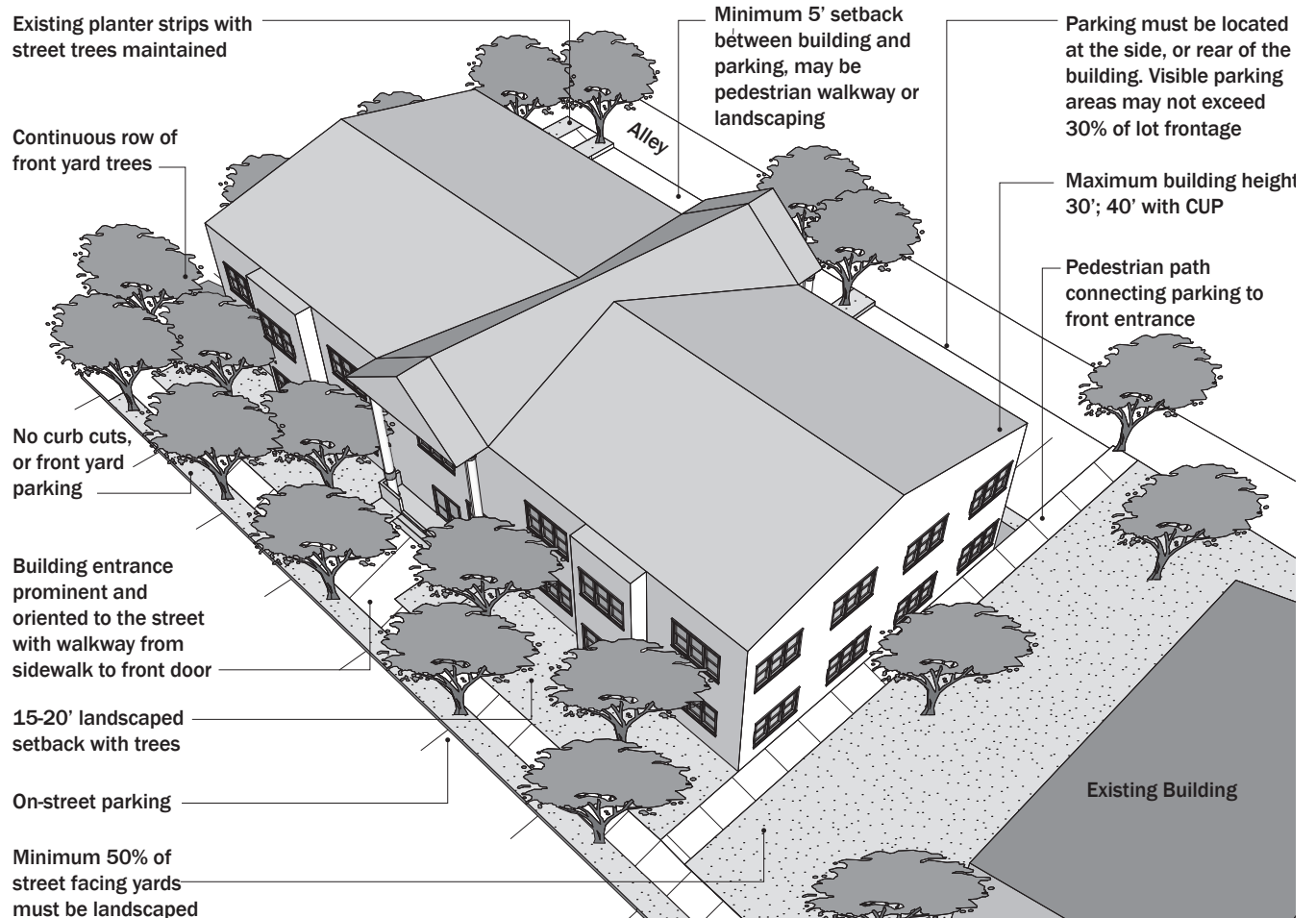


Street trees shade the sidewalk. Front yards are landscaped.



Renovations of houses for office use remain consistent with the character of the neighborhood.

Figure A5-2:
Downtown Professional Office: Building Form - New Construction



Downtown Professional Office: New Construction: Building Design and Exterior Materials

- Incorporate all the key elements of building form and street design for the D-PO District, as listed in this section.
- Design buildings to include a pitched roof, consistent with the form of the district.
- Break larger buildings down into smaller masses in order to be consistent with the small scale of existing buildings. This can be accomplished with changes in height, stepbacks, setbacks, vertical articulation, bay windows, front porches, and other elements that break up the building mass into smaller components.
- Pattern new buildings after one of the historic architectural styles found in the residential buildings in the D-PO District.
- Design buildings to be representative of only one residential style. It may not be a mixture of different styles.
- Design the new building to incorporate the massing, scale, shape, height, proportions, and materials of the historic style that has been selected.

Street Design – Key Features:

- Maintain streets as local two-lane streets, with parking on both sides.
- Maintain planter strips between the curb and the sidewalk where they exist.
- Maintain existing street trees that line the street, typically in a planter strips, and install new trees where needed to create a continuous row.
- Add a continuous row of trees in landscaped front yard setback areas.

Figure A5-3:
Downtown Professional Office: D Street Enhancements

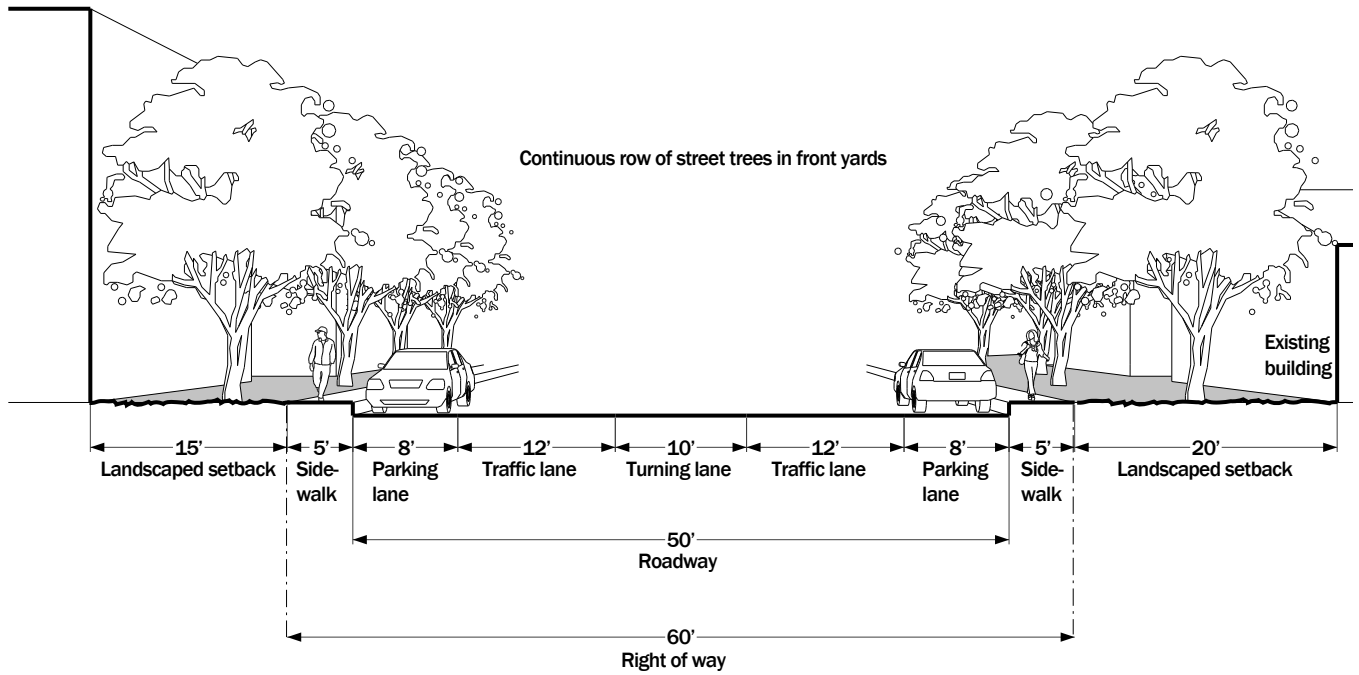


Figure A5-4:
Downtown Professional Office: 2nd Street Enhancements

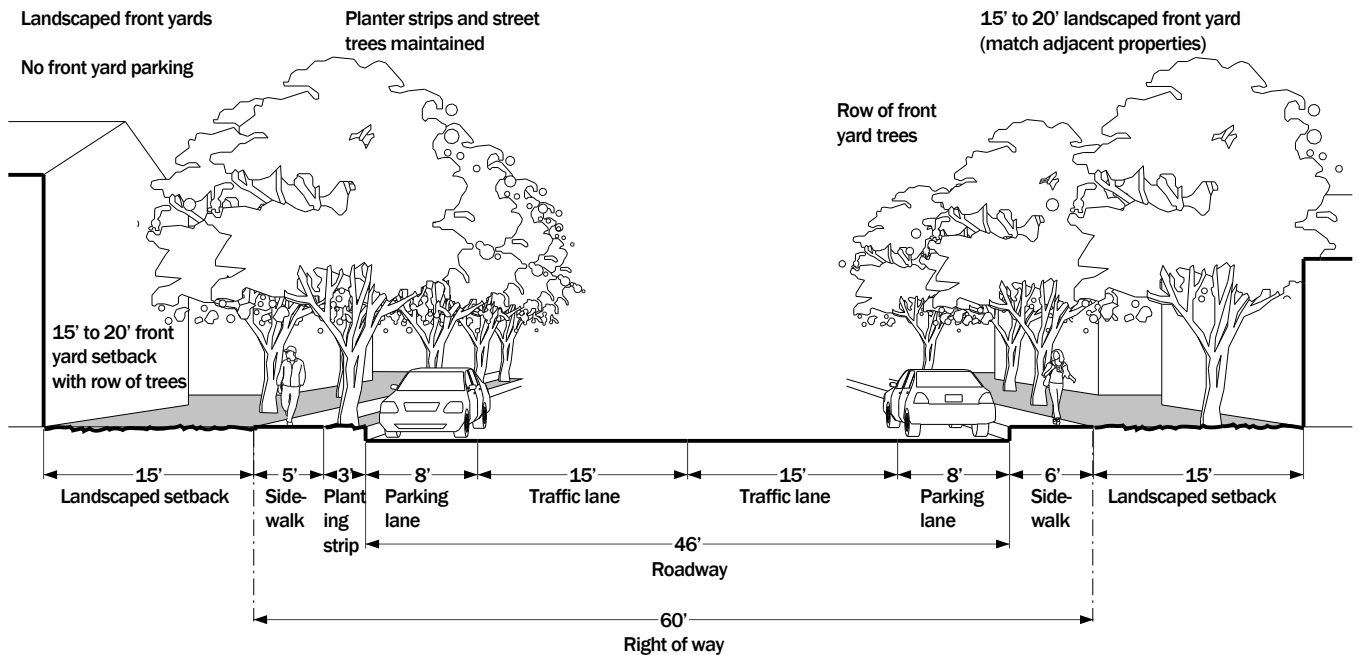


Figure A5-5:
Downtown Professional Office: Putnam Street Enhancements

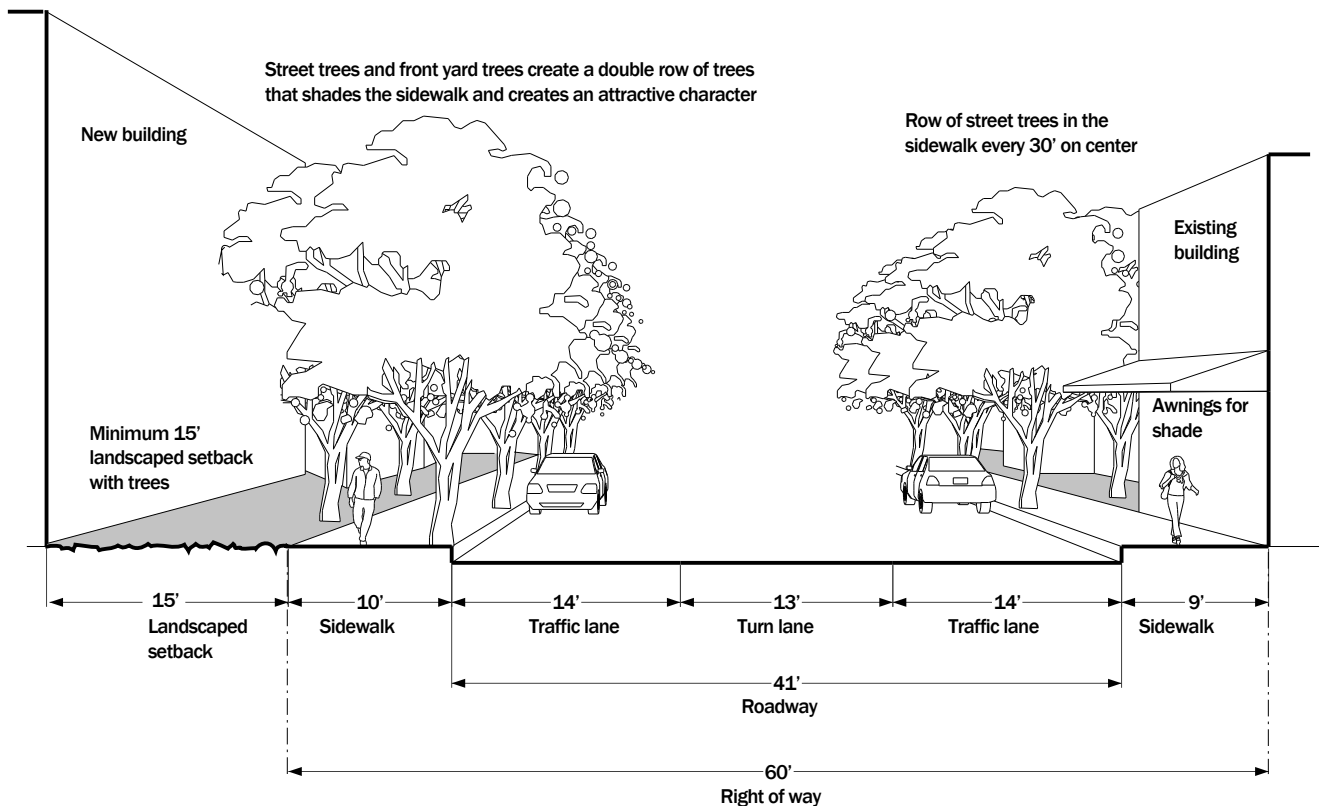
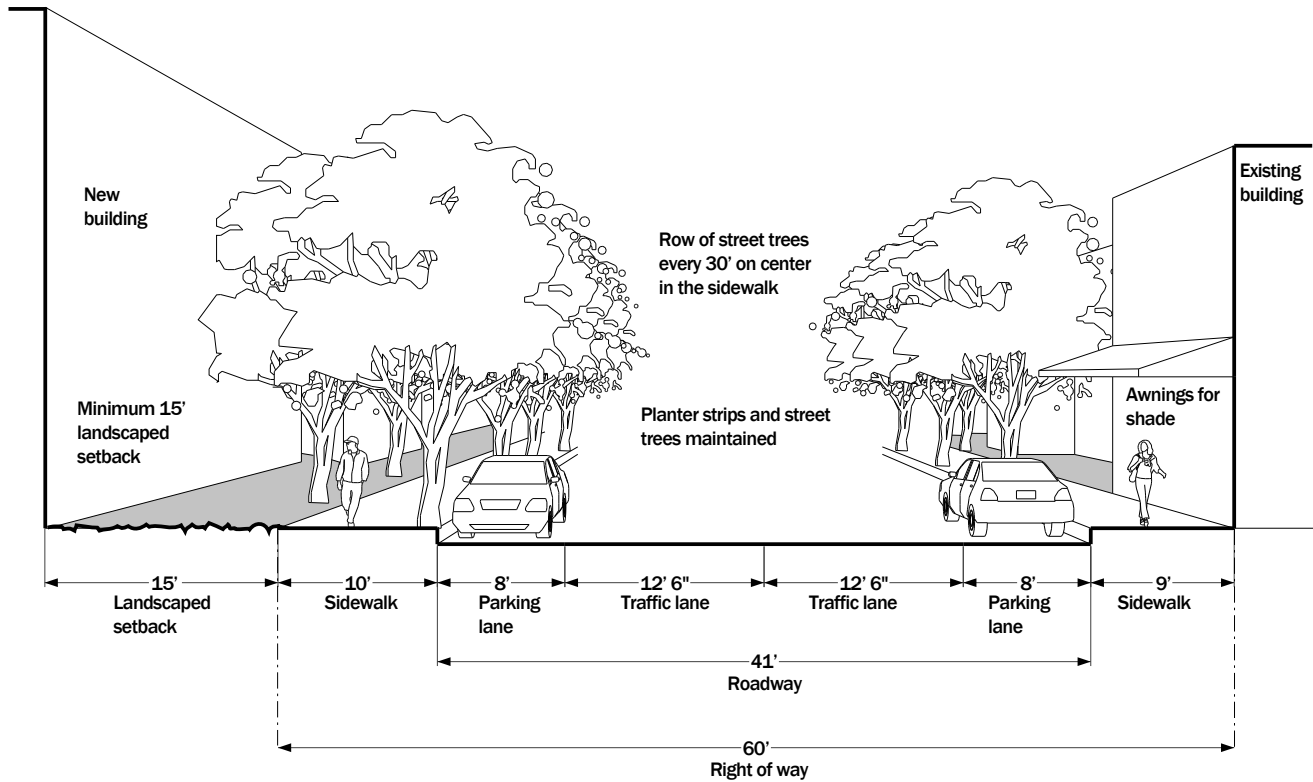


Figure A5-6:
Downtown Professional Office: Thurman & Cleveland Street Enhancements

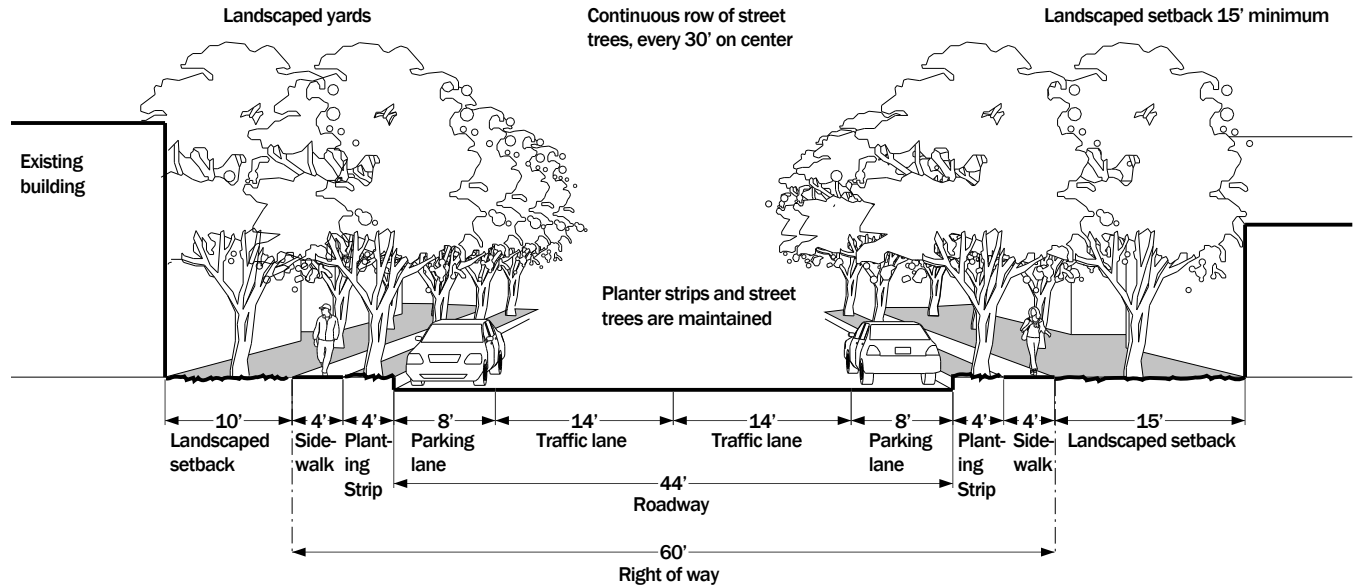
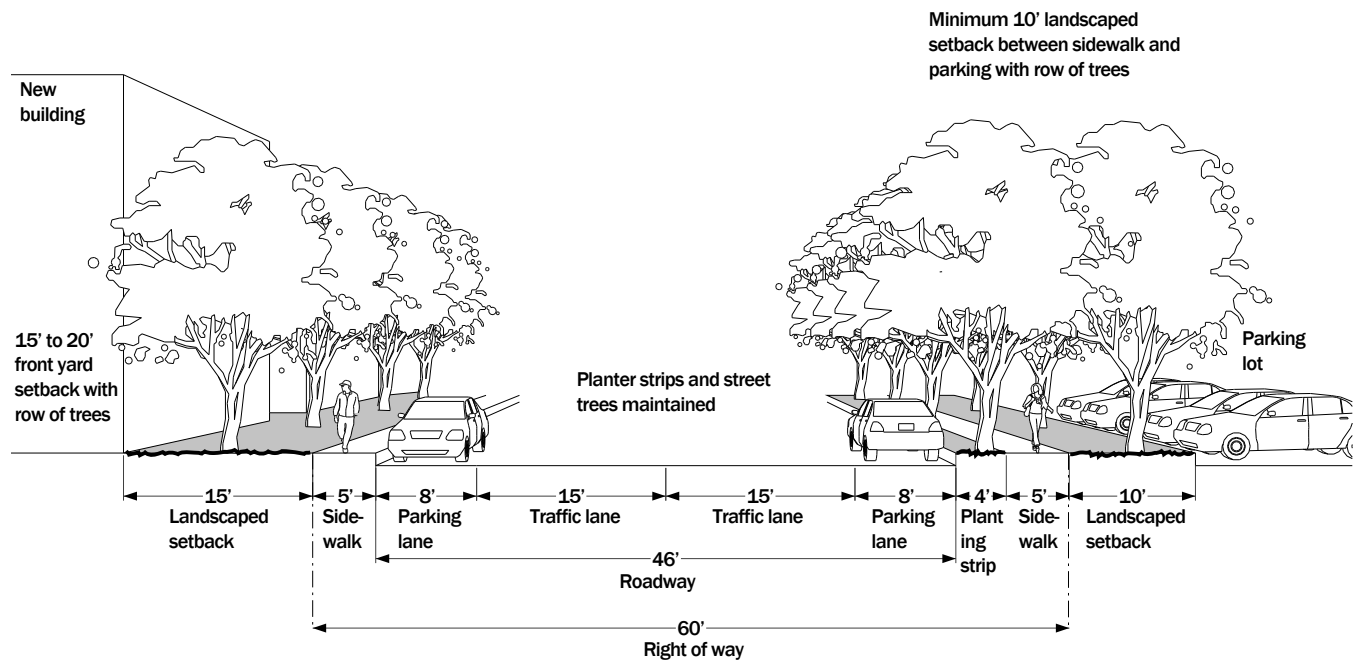
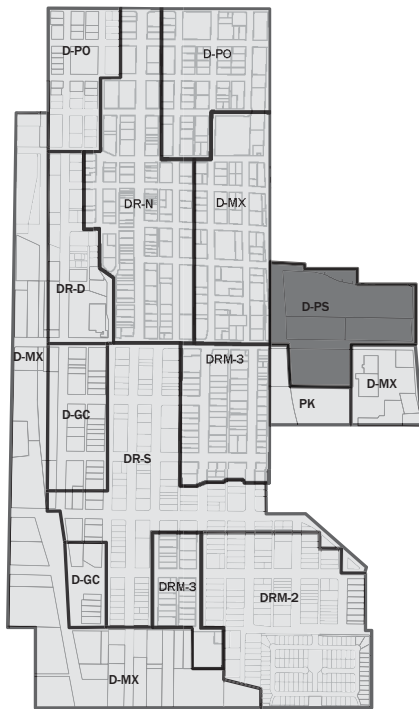


Figure A5-7:
Downtown Professional Office: 3rd & 4th Street Enhancements





6. DOWNTOWN PUBLIC AND SEMI-PUBLIC (D-PS)

Purpose:

Provide areas for larger-scale public facilities that serve the community, with a focus on integrating development into the Downtown. Encourage land uses and design features that enhance the vitality, character and quality of life of the Downtown and surrounding residential areas. Ensure that the traditional block pattern of Downtown is maintained. Incorporate public streets and pedestrian walkways that connect these larger scale projects to Downtown streets and walkways.

Character:

- Design streets with attractive landscaped front yards and street trees, consistent with the character of existing and adjoining streets.
- Ensure street connectivity consistent with the Downtown area.

Development Density and Building Heights:

- Maximum FAR: 0.25, where meeting rooms, cafeteria facilities, and conference facilities are excluded from floor area.
- Maximum Building coverage: 50 percent of lot
- Maximum Building Height:
 - 50 feet (3-4 stories) on corner lots and lots over 2 acres
 - 40 feet (1-3 stories) on interior lots

Building Form – Key Features:

- Orient buildings toward major streets, including Plano and Olive.
- Create a 20 foot landscaped front yard between the building and the sidewalk.
- Locate parking behind buildings, so that parking does not occupy more than 30 percent of the street frontage.

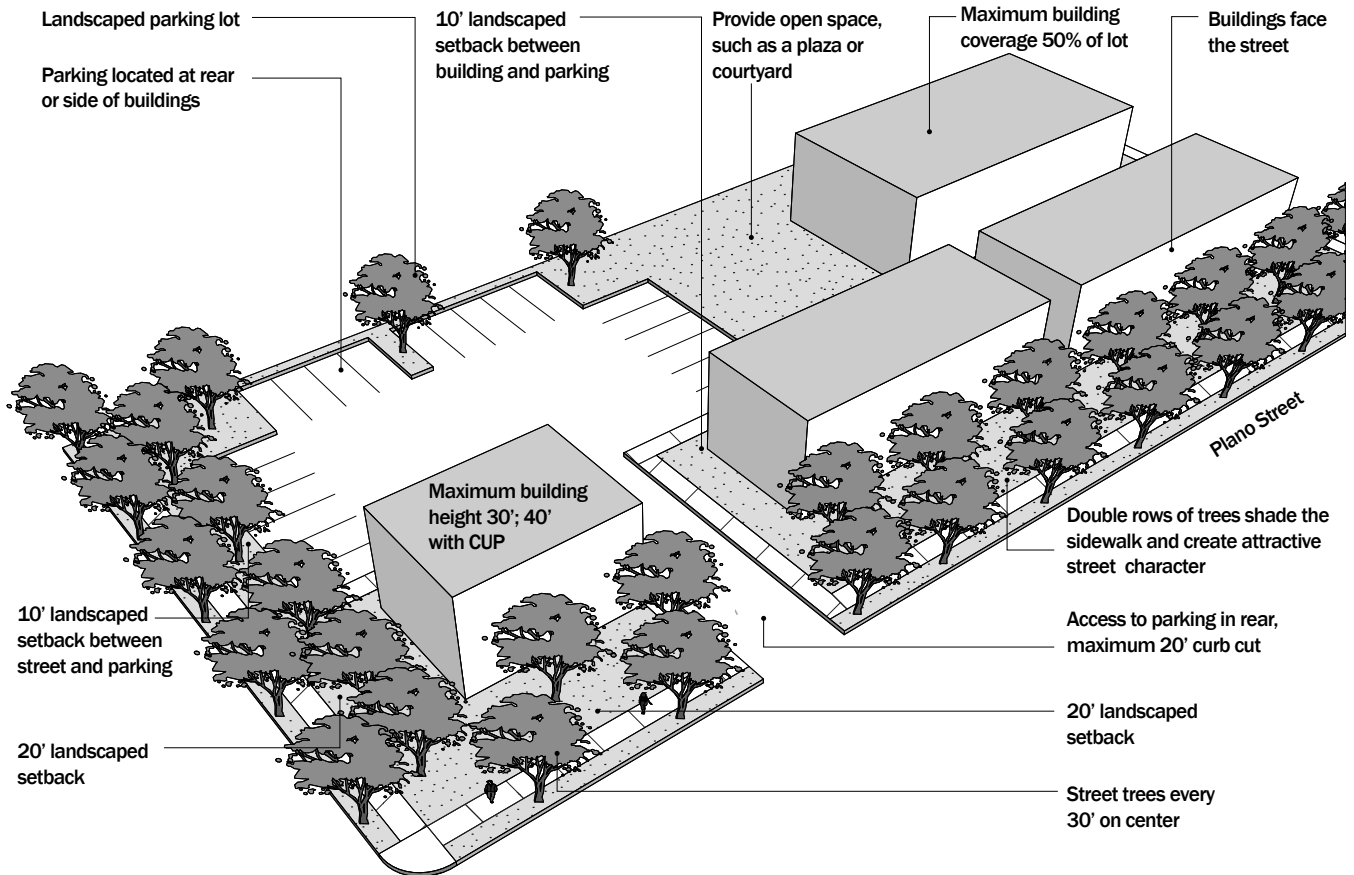
Street Design – Key Features:

- Include pedestrian walkways for blocks exceeding 500 feet. Blocks may not exceed 600 feet.
- Provide street and pedestrian connections to the Downtown area.
- Include pedestrian walkways, plazas, and courtyards within large projects.
- Line streets with continuous rows of street trees, and include a continuous row of trees in the landscaped front yard, to create a double row of trees.
- Provide continuous bike lanes on Olive Avenue.
- Maintain the landscaped median on Plano Street.



Buildings shall be set back from the street by 20 feet.

Figure A6-1:
Downtown Public and Semi-Public: Building Form



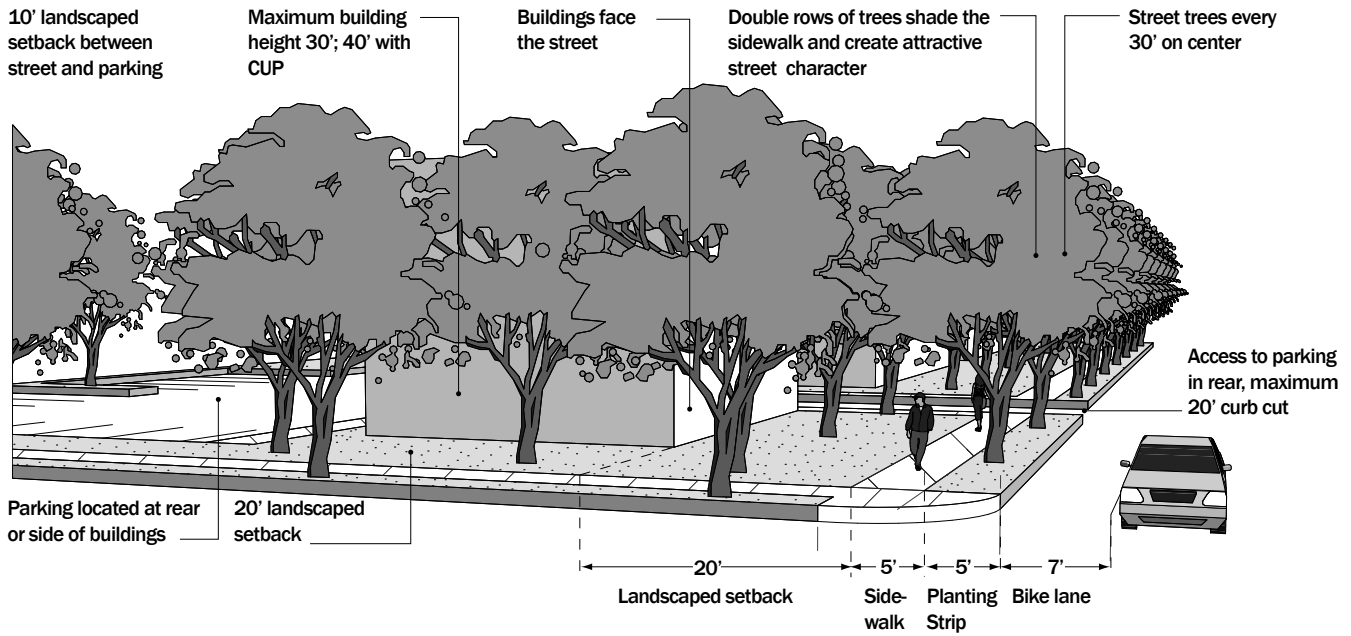
The site shall have a 20 foot landscaped front yard setback lined with trees.



The existing greenway runs along the DPS area and provides additional pedestrian access to the site.

Figure A6-2:

Downtown Public and Semi-Public: Building to Street Relationship on Olive and Plano



DOWNTOWN MEDIUM DENSITY RESIDENTIAL

7. DOWNTOWN MEDIUM DENSITY RESIDENTIAL (DRM-2)

Purpose:

Encourage a variety of housing types, such as small-lot single-family homes, detached zero lot line developments, duplexes, townhouses, and garden apartments with a maximum residential density of 16.5 units per gross acre in the Downtown area. Encourage housing in Downtown to provide opportunities for residents to live in close proximity to community services and retail establishments, and to help support the vitality of Downtown businesses.

Character:

Maintain the existing character of single family homes and other small residential buildings that front the street with landscaped front yards, planting strips, and street trees.

Development Density and Building Heights:

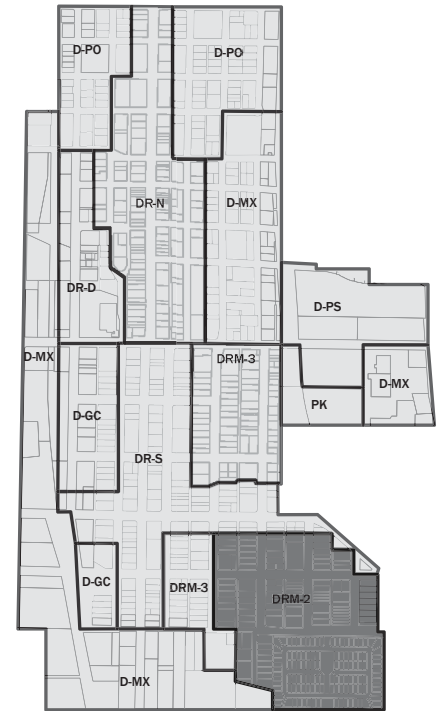
- Maximum Building coverage: 50 percent of lot
- Maximum Dwelling Units per Acre: 16.5
- Maximum Building Height: 35 feet

Building Form – Key Features:

- Face residential units toward the street. Include front doors, windows, and porches that face the street.
- Provide front yards that are 20 feet deep and landscaped, with at least 50 percent of the yard landscaped.
- Locate parking behind or at the side of buildings.
- Do not locate parking in front yards.

Street Design – Key Features:

- Preserve planter strips and street trees.
- Create a double row of trees by planting trees in front yards to shade the sidewalks, where feasible, in addition to trees along the sidewalk.

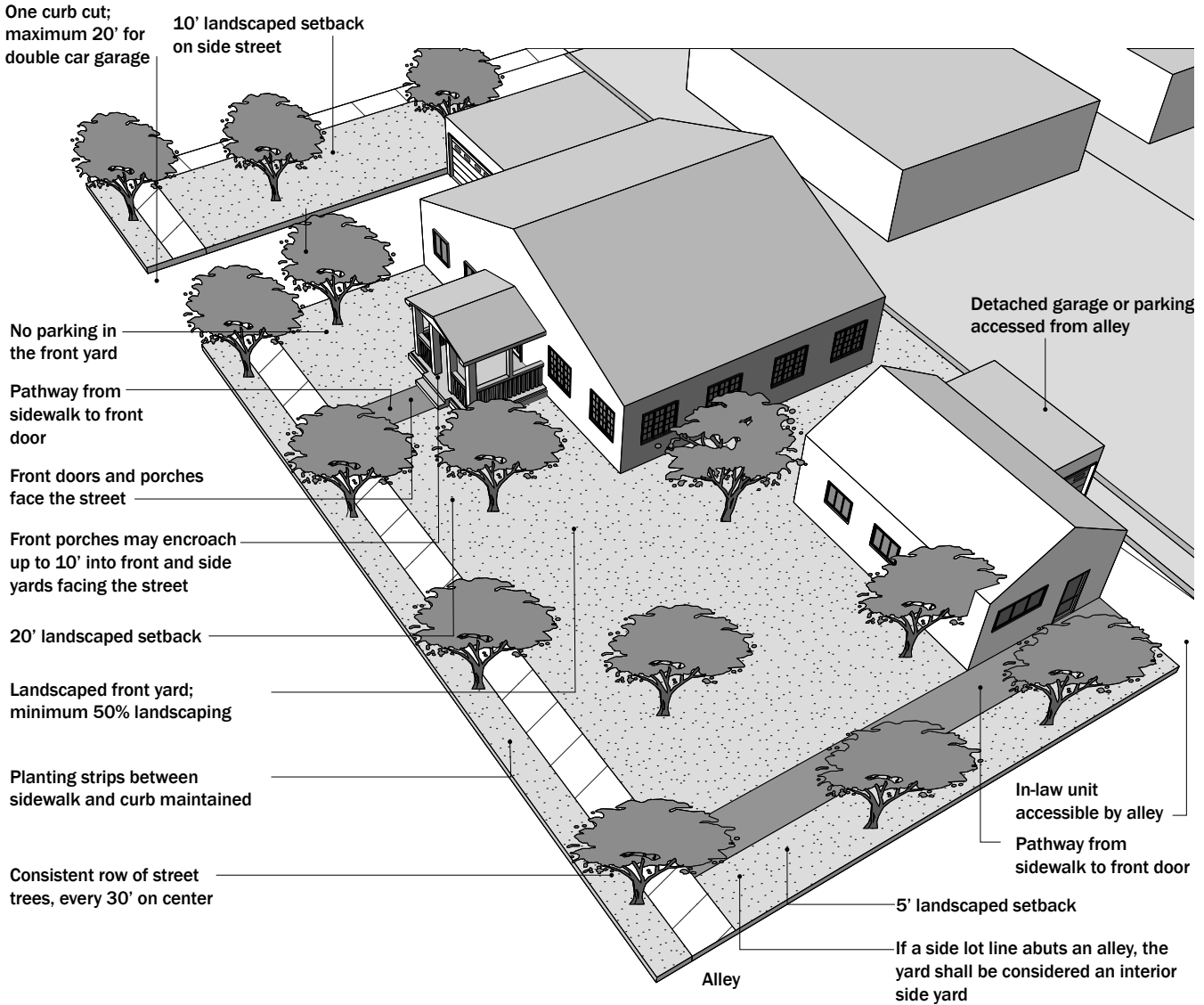


Typical buildings are residential homes of 1-2 stories with 20 foot landscaped front yards.



Typical streets are two-lane local streets with parking on both sides, planter strips, and a consistent row of street trees.

Figure A7-1:
Downtown Medium Density Residential: Building Form



Where street trees are not provided in a planting strip, front yards should provide trees to shade the sidewalk.



Do not locate parking in front of buildings.

DOWNTOWN HIGH DENSITY RESIDENTIAL

8. DOWNTOWN HIGH DENSITY RESIDENTIAL (DRM-3)

Purpose:

Facilitate the development of attached homes, two- to four-plexes, and apartment or condominium buildings with a maximum residential density of 33.0 units per gross acre in Downtown to create a more vibrant community and offer additional opportunities for housing for residents requiring convenient access to community services and less dependency on auto transportation.

Character:

Maintain the existing character of residential buildings that front the street with landscaped front yards, and street trees.

Development Density and Building Heights:

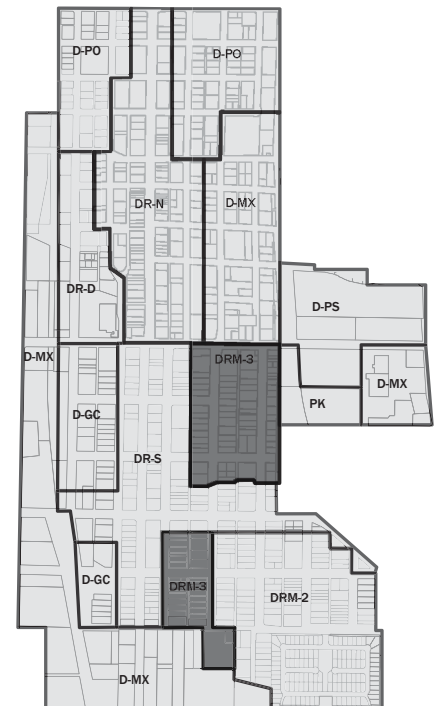
- Maximum Building coverage: 60 percent of lot
- Maximum Dwelling Units per Acre: 33.0
- Maximum Building Height:
 - 40 feet (1-3 stories)
 - 50 feet on corner lots (3-4 stories)

Building Form – Key Features:

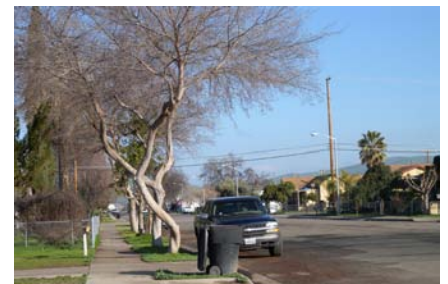
- Face residential units toward the street. Include front doors, windows, and porches that face the street.
- Provide front yards that are 15 feet deep and landscaped, with at least 50 percent of the yard landscaped.
- Locate parking behind or at the side of buildings.
- Do not locate parking in front yards.

Street Design – Key Features:

- Preserve planter strips and street trees.
- Create a double row of trees by planting trees in front yards to shade the sidewalks, where feasible, in addition to trees along the sidewalk.

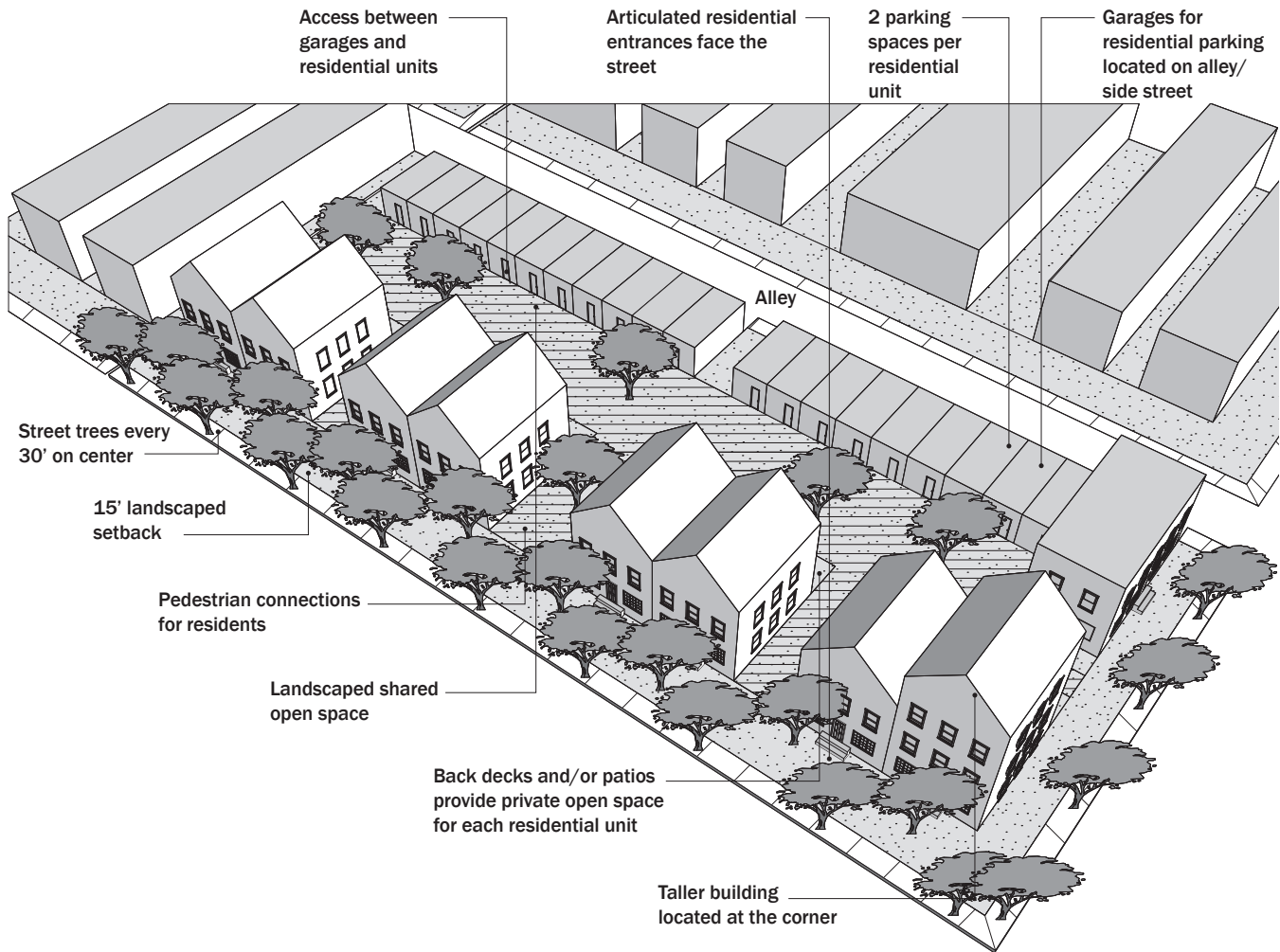


Typical buildings are residential homes of 1-3 stories that face the street and have 15 foot landscaped front yards.



Typical streets are two-lane local streets with parking on both sides, planter strips, and a consistent row of street trees.

Figure A8-1:
Downtown High Density Residential: Building Form



The high density residential block depicted here and in the site plan on the following page has an overall density of 24 units per acre. The block is made up of seven 1,500 square foot two story townhouses, a 2,400 square foot three story townhouse, and a 1600 square foot apartment located at the back of the block. There are private back decks or patios for each unit as well as 9,600 square feet of common open space, which exceeds the required 150 square feet per unit.

DOWNTOWN HIGH DENSITY RESIDENTIAL

Figure A8-2:
Downtown High Density Residential: Building Form and Site Planning

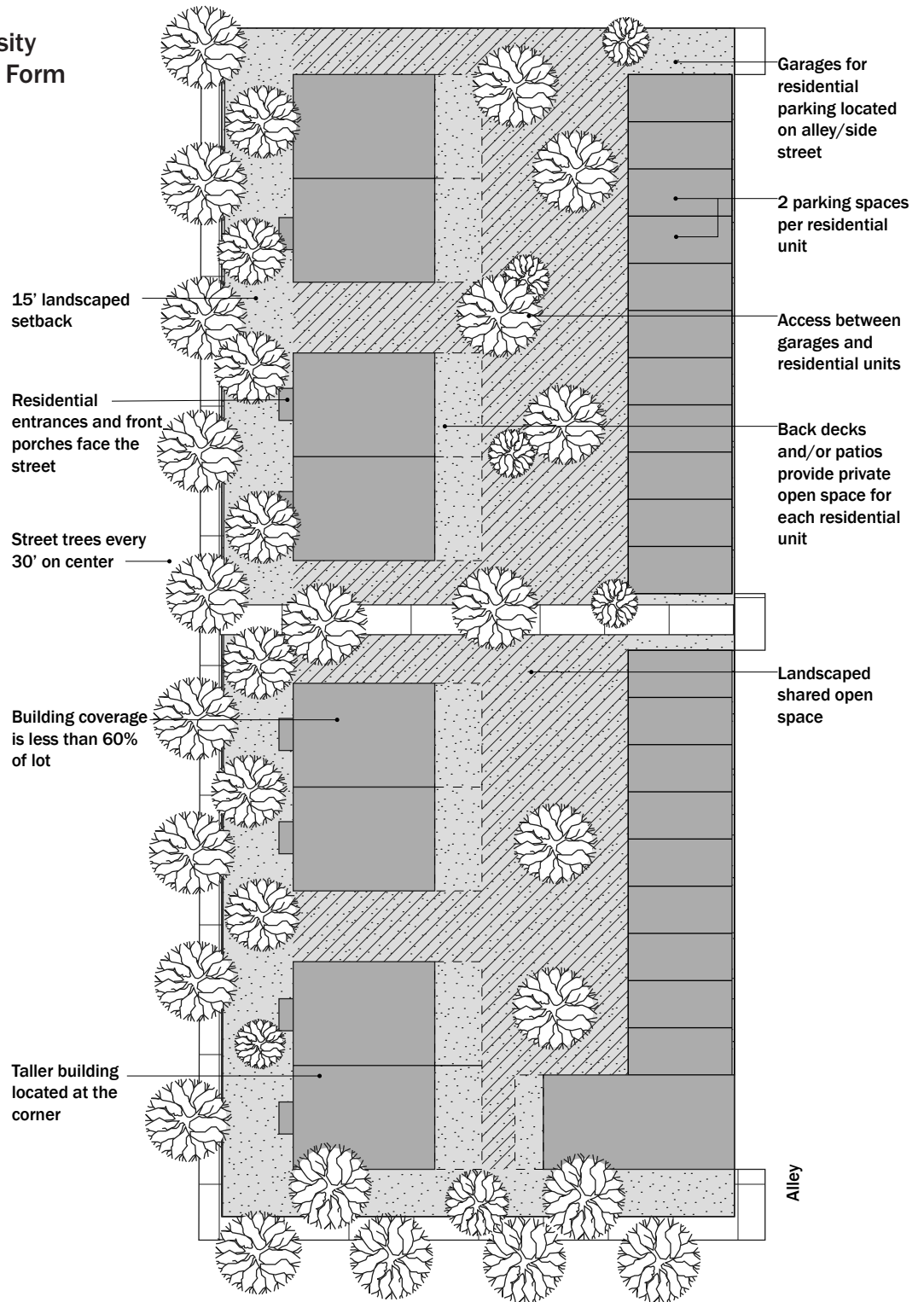


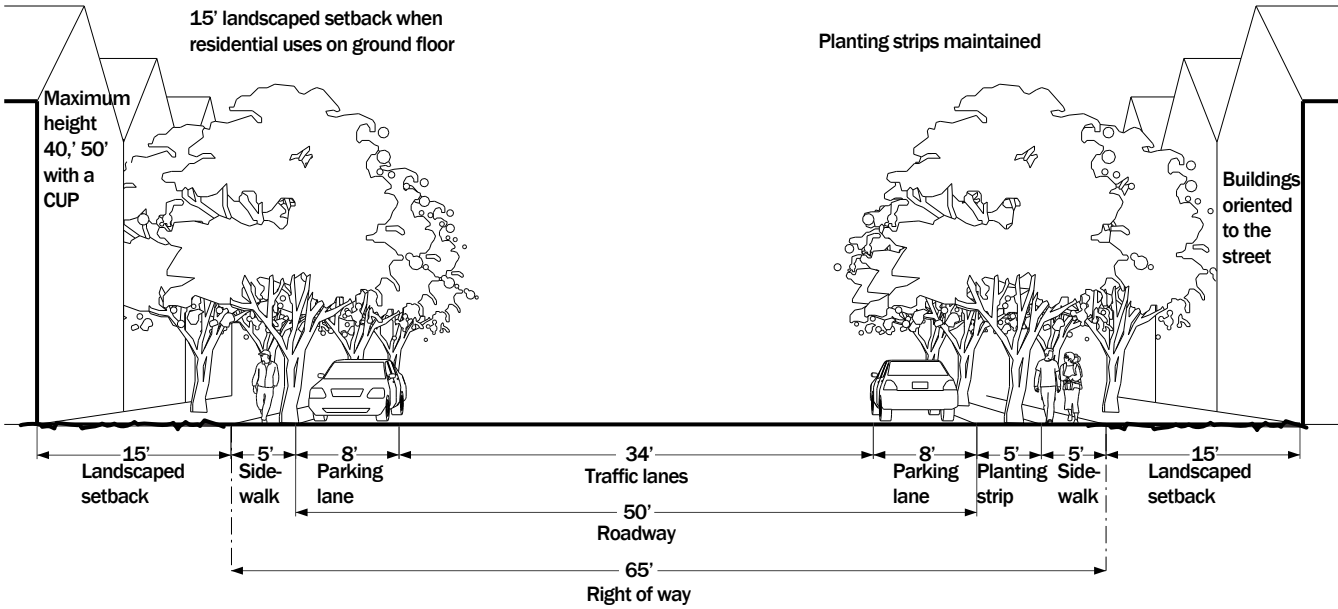
Figure A8-3:
Typical Street Enhancements (B Street)

Trees are located in grates if no planting strip exists and the sidewalk is wide enough to accommodate the grate and sufficient pedestrian and handicapped access

Double rows of trees shade the sidewalk and create attractive street character

Street trees every 30' on center

Planting strips maintained



Typical streets in DRM-3 include landscaped setbacks, 5' wide sidewalks, planting strips, and two lane streets with parking on both sides.



A continuous row of street trees along B Street.

DOWNTOWN GENERAL COMMERCIAL

9. DOWNTOWN GENERAL COMMERCIAL (D-GC)

Purpose:

Provide areas for retail, service, and heavier commercial uses that meet local and community needs in a manner complimentary to the adjacent Downtown pedestrian-oriented environment.

Character:

Establish a service and commercial district at the edge of the Downtown area, consistent with the pedestrian-oriented Downtown.

Development Density and Building Heights:

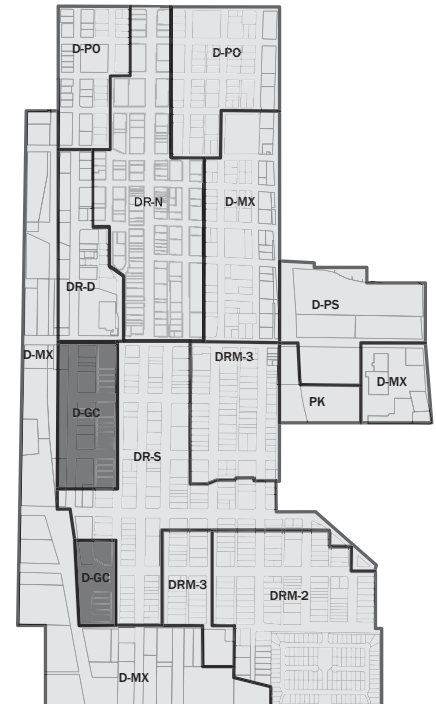
- Maximum Floor Area Ratio: 1.0
- Maximum Dwelling Units per Acre: n/a
- Maximum Building Height:
 - 50 feet (3-4 stories) on corner lots and lots over 2 acres
 - 40 feet (1-2 stories) on interior lots

Building Form – Key Features:

- Orient buildings to face the street with 10 foot setbacks.
- Design buildings to range from 1-3 stories and locate taller buildings on the corners of blocks.

Street Design – Key Features:

- Provide street trees for shade and definition of sidewalk edges.

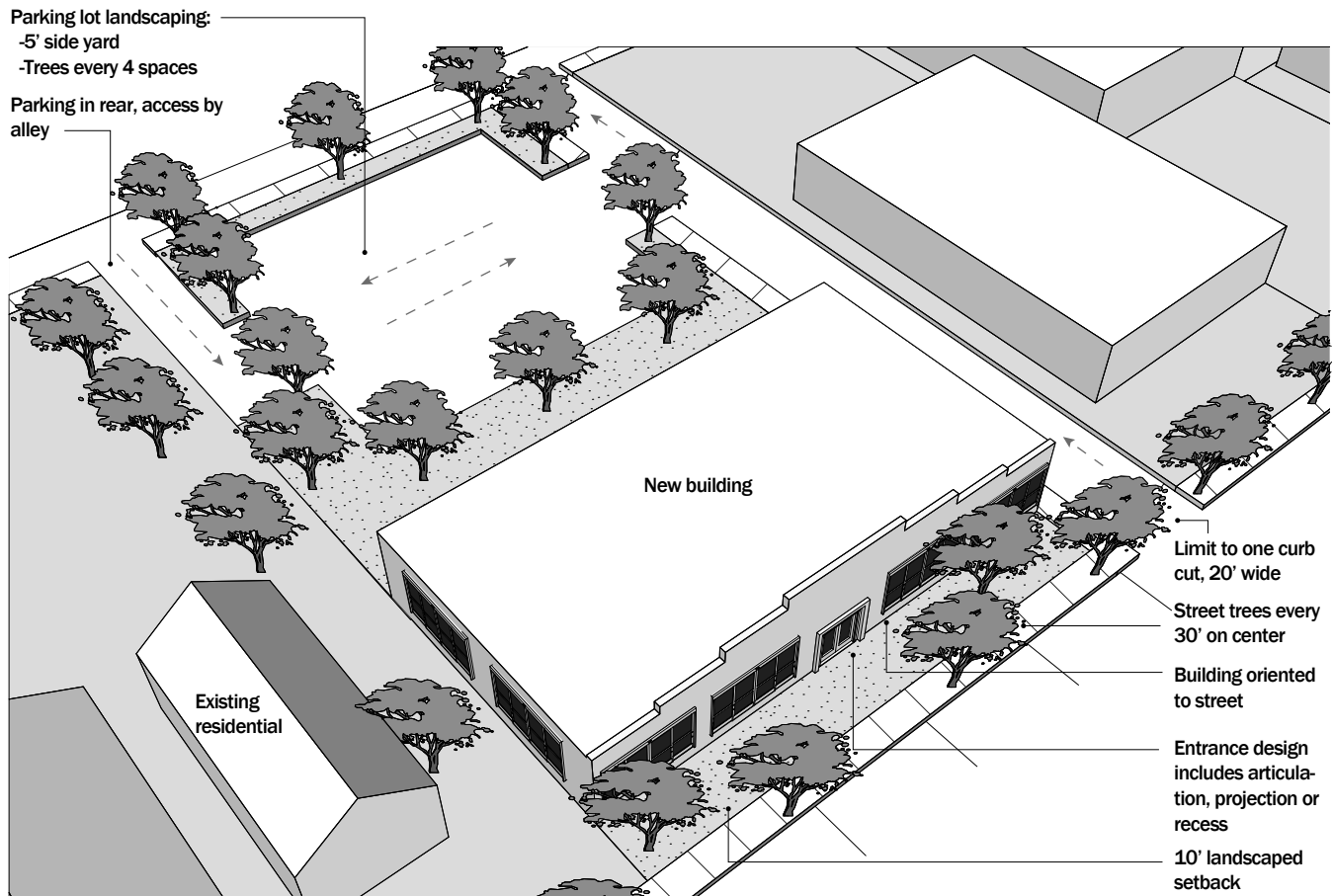


Typical buildings are one story commercial and service, lining the street or set back up to 10 feet.



Typical streets are two-lane local streets with parking on both sides.

Figure A9-1:
Downtown General Commercial: Building Form

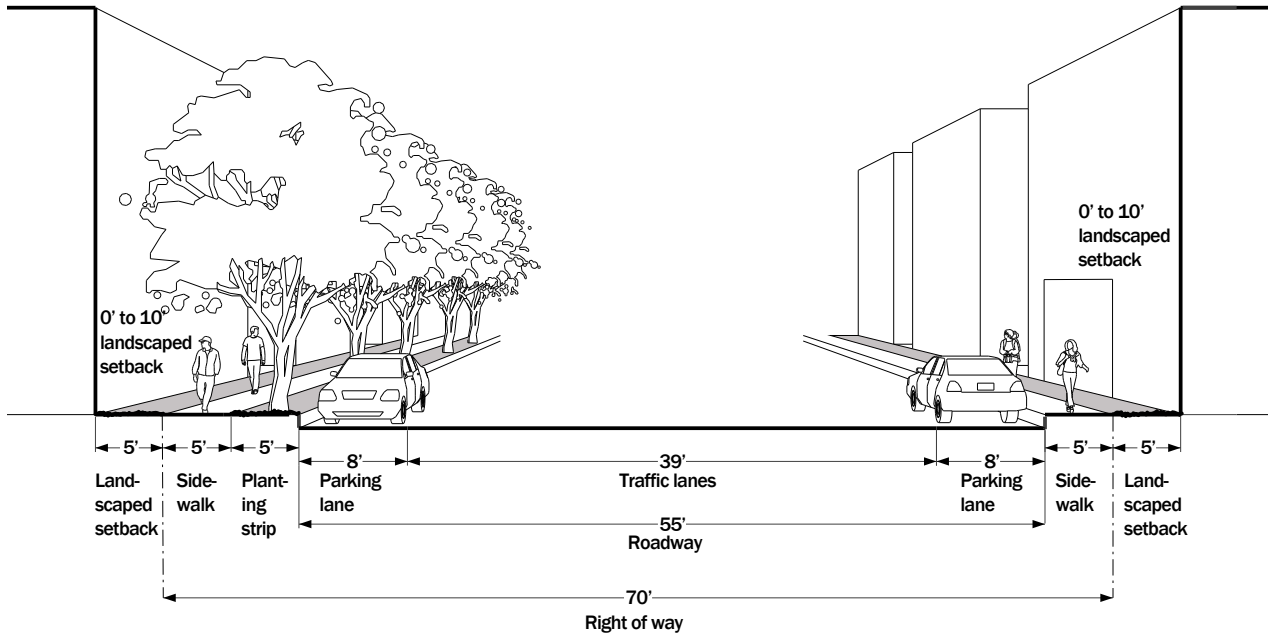


Move parking to rear or off of alleys or side streets where feasible and add street trees. Hockett Street provides access to parking that serves Olive Avenue.



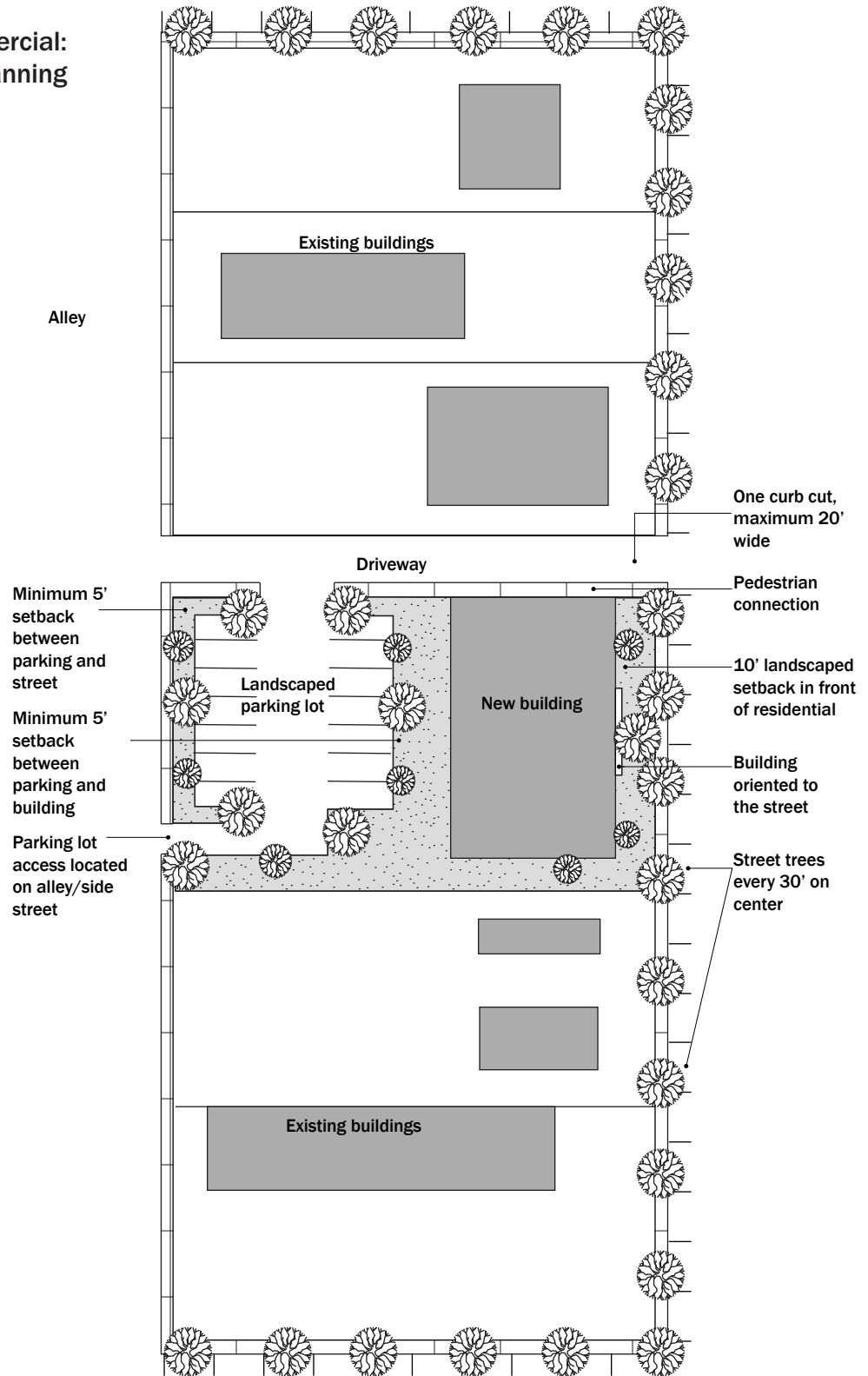
Where feasible, do not locate parking between the building and sidewalk.

Figure A9-2:
Hockett Street Enhancements



Provide additional street amenities such as street trees to make the area more pedestrian friendly.

Figure A9-3:
**Downtown General Commercial:
Building Form and Site Planning**



B. BUILDING DESIGN

1. EXTERIOR BUILDING MATERIALS AND COLORS

A unified palette of materials and colors shall be used on all sides of buildings regardless of orientation.

Quality Materials

Exterior building materials shall be brick, stucco, concrete block, painted wood clapboard, painted metal clapboard or other quality, durable materials approved by the Zoning Administrator. A unified palette of materials shall be used on all sides of buildings.

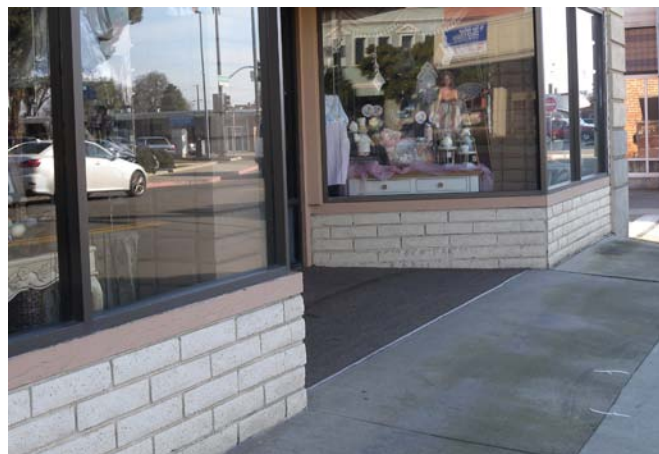
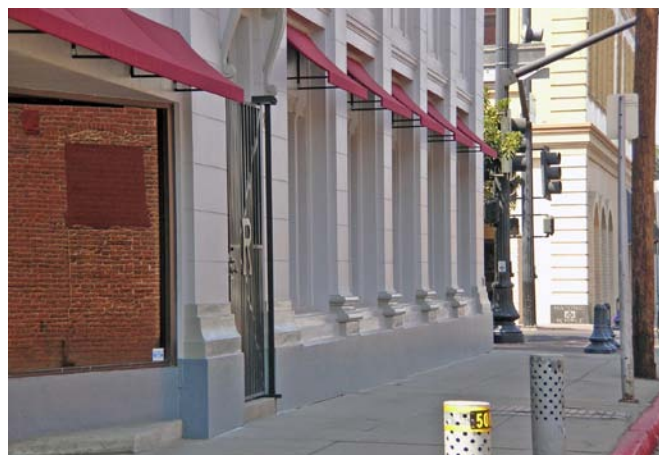
Building Base

Buildings shall include a base (bulkhead) between the sidewalk and building windows. The building base shall be tile, stone, brick, concrete, or other comparable durable material; stucco is prohibited.

Figure B1-1:
Quality Materials



Figure B1-2:
Building Base



Wall Surface - Inappropriate

The exterior use of slump block, board and batten, dark reflective glass, galvanized metal or unpainted metallic surfaces, and wood or composition shingles is prohibited.

Exterior Colors

Colors shall reflect the historic character of Downtown. Fluorescent paints, blues, purple, bright red or orange, pink, and bright green are prohibited on Main Street. Generally avoid highly saturated hues.

Figure B1-3:

Wall Surface – Inappropriate

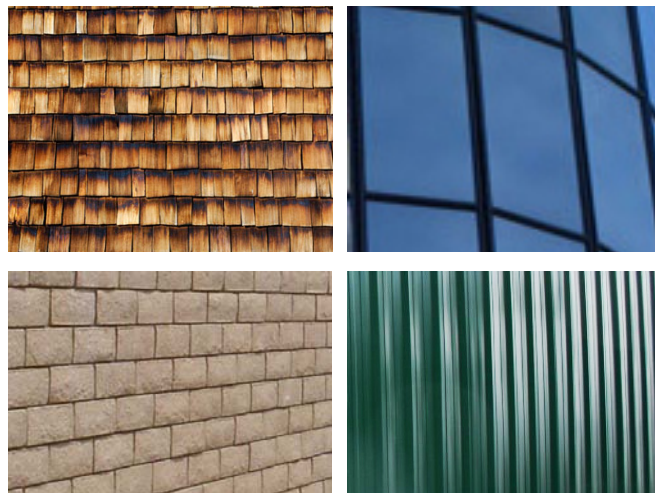


Figure B1-4:

Colors - Inappropriate



2. GROUND FLOOR WINDOWS AND DESIGN

Façades facing streets, plazas, and courtyards shall be lined with windows. Where blank walls are necessary, they shall be on side or interior façades not facing streets. Exterior walls facing an adjacent public street, or facing onto a park, plaza, or other public outdoor space shall include windows, doors, or other openings for at least 50 percent of the building wall area located between 2.5 and seven feet above the elevation of the sidewalk.

Figure B2-1:
Ground Floor Windows and Design



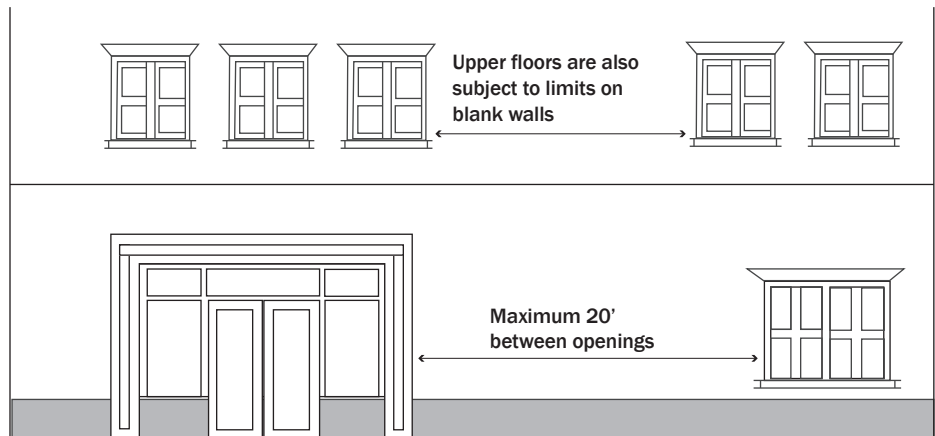


Typical Main Street frontage includes regular openings.

3. LIMITS ON BLANK WALLS

No wall shall run in a continuous plane for more than 20 feet without an opening on Main Street and Olive Avenue, and for more than 30 feet on other streets. Openings fulfilling this requirement shall have transparent glazing and provide views into work areas, display areas, sales areas, lobbies, or similar active spaces, or into window displays that are at least three feet deep.

Figure B3-1:
Limits on Blank Walls



Walls must have openings at least every 20' on Main Street and Olive Avenue, and every 30' on other streets. Openings shall have transparent glazing and provide views into active spaces.

4. COMMERCIAL GROUND FLOOR ARTICULATION

Commercial building façades shall include building projections or recesses, doorway and window trim, and other details that provide architectural articulation and design interest.

Windows, doors, columns, and other features shall be recessed or project forward from the wall plane, such that there is a minimum two-inch recess of the window plane from the wall plane, a minimum four-inch projection from the window plane and outer edge of trim, and a total of at least 18 inches from the window plane to the outermost plane of a wall, column, or other articulation feature.

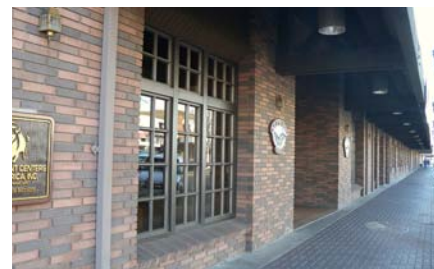
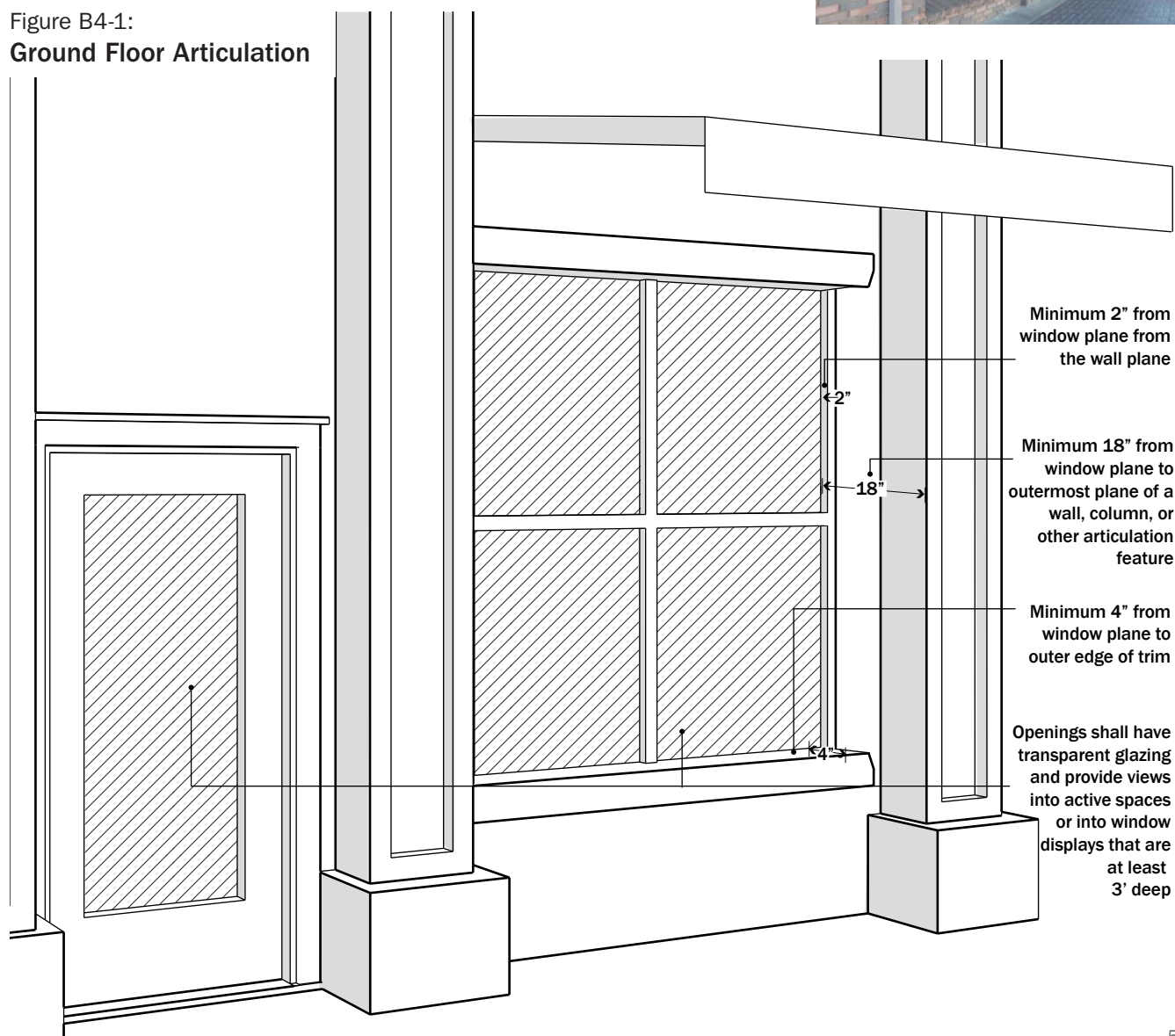


Figure B4-1:
Ground Floor Articulation



5. BUILDING PROJECTIONS

Front Porches and Building Entrance Structures

Front porches and building entrance structures may encroach up to 10 feet into front and side yards facing the street for up to 40 percent of the lot frontage.

Architectural Projections

Bay windows, balconies, and other architectural features such as chimneys may project into required yards up to three feet.

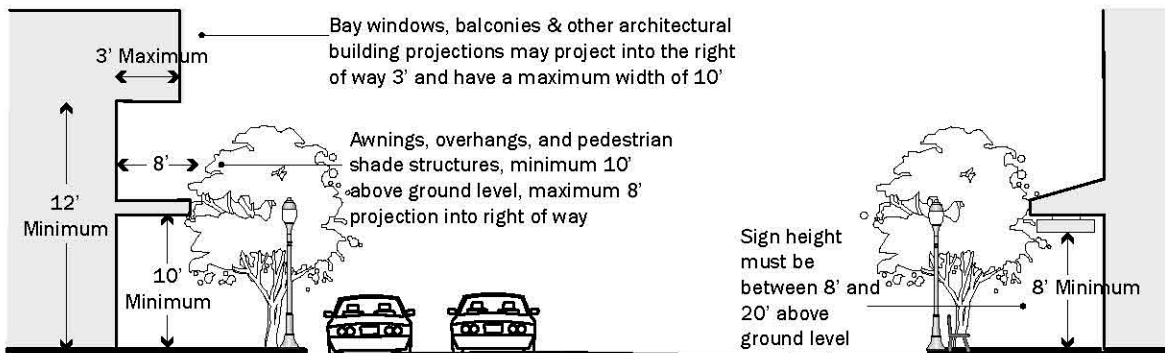
Building Projections into the Public Right-of-Way

Bay windows, balconies, and other architectural building projections may encroach into the public right-of-way up to three feet for the purpose of providing shade over the sidewalk, provided they are no closer than 12 feet from sidewalk grade. Maximum width of any projection is 10 feet.

Awnings and Overhangs

Awnings, overhangs, and pedestrian shade structures may encroach into the public right-of-way up to eight feet for the purpose of providing shade over the sidewalk, provided they are at least 10 feet above sidewalk grade.

Figure B5-1:
Building Projections



6. RESIDENTIAL BUILDING ARTICULATION

All multiple-family residential buildings and mixed-use buildings that include residential uses shall include adequate design features to create visual variety and avoid a large-scale and bulky appearance. Long façades shall be broken up into smaller modules. This requirement can be met by using two or more of the following methods.

- *Façade articulation:* All street-facing façades have at least one horizontal or vertical projection or recess at least four feet in depth, or two projections or recesses at least 2.5 feet in depth, for every 25 horizontal feet of wall. If located on a building with two or more stories, the articulated elements shall be greater than one story in height, and may be grouped rather than evenly spaced in 25-foot modules. Building entrances and front porches and projections into required yards such as stoops, bays, overhangs, fireplaces, and trellises may count towards meeting this requirement.
- *Variable roof form:* Variable roof forms are incorporated into the building design, and no more than two side-by-side units are covered by one unarticulated roof. Articulations may be accomplished by changing roof height, offsets, and direction of slope, and by introducing elements such as dormers, towers, or parapets.
- *Façade detailing and materials:* All visible building façades incorporate details, such as window trim, window recesses, cornices, changes in materials or other design elements, in an integrated composition. Each side of a building that is visible from a public right-of-way shall be designed with a complementary level of detailing and quality of materials.
- *Use of balconies, bay windows, and other such projections or recesses:* The building incorporates balconies, bay windows, entry porches or other projections and recesses in a pattern that creates architectural interest across the length of the façade.



Façade articulation, use of porches and balconies, and variable roof form.



Use of balconies and variable facade depth.



Use of bay windows, variable roof height, and facade detailing.

7. BUILDING ENTRANCES

Commercial

- Buildings shall be oriented to face public streets.
- Building frontages shall be generally parallel to streets, and the primary building entrances shall be located on a public street.
- Building entrances shall be emphasized with special architectural and landscape treatments.
- Rear entry doors, door frames, door hardware, signs and lighting fixtures should match types utilized on the front façade and/or that are appropriate to the parent building's historic period.
- In mixed-use developments, entrances to residential units shall be physically separated from the entrance to the permitted commercial use and clearly marked with a physical feature such as a recess or projection incorporated into the building or appropriately scaled element applied to the façade.

Figure B7-1:
Commercial Entrances



All commercial buildings and primary building entrances shall be oriented toward public streets.

Figure B7-2:
Mixed Use Entrances



Entrances to residential units shall be physically separated from the entrance to the commercial use and clearly marked.



The entrance to retail uses are separate from the entrance to offices on the second floor, located on the side street, similar to how residential access should be separated.

Residential

- All units located along public rights-of-way shall have the primary entrance facing this right-of-way. Exceptions to this requirement shall only be approved for projects where multiple-family housing is located on four-lane streets carrying high traffic volumes and/or streets that do not allow on-street parking. In such cases, the project shall be oriented around courtyards.
- Street-facing façades of residential buildings shall be designed as front façades and shall include front doors, windows, stoops, porches, recessed windows, and bay windows or balconies.
- Building entrances shall have a roofed projection (such as a porch) or recess with a minimum depth of at least five feet and minimum area of 50 square feet. Alternative designs that create a welcoming entry feature facing the street, such as a trellis or landscaped courtyard entry, may be approved.
- Provide at least one building entrance for every 100 feet of street frontage.

Rear

Rear entry doors, door frames, door hardware, signs and lighting fixtures shall match types utilized on the front façade and/or that are appropriate to the parent building's historic period.

Figure B7-3:

Residential Entrances

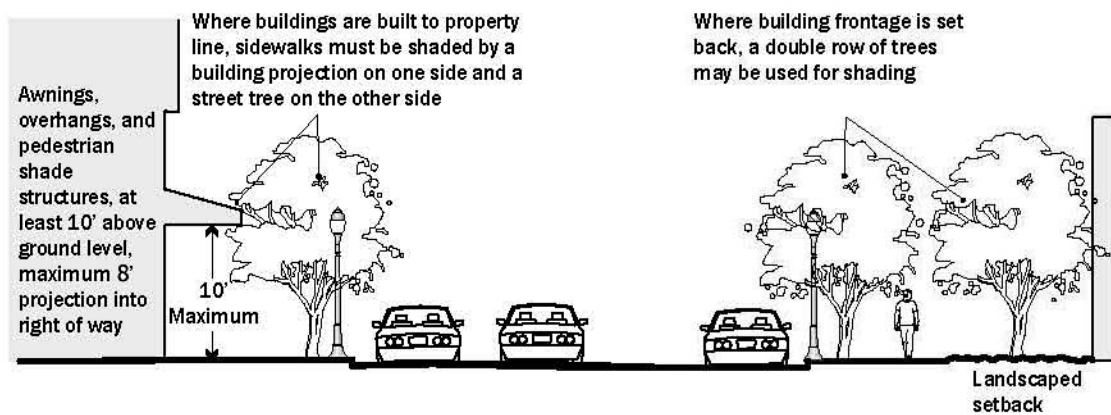


All residential buildings and primary building entrances shall be oriented toward public streets.

8. SHADE

On key pedestrian streets a minimum of 50 percent of the sidewalk shall be shaded. Shade shall be provided by a double row of trees or a building projection (awning, overhang, or pedestrian shade structure) on one side and a street tree on the other side. In areas where 100 percent of the building is at the property line, this requirement can only be met by a building projection on one side and a street tree on the other side.

Figure B8-1:
Shade



Double rows of trees, planted along the sidewalk and in a landscaped setback, provide shade for the sidewalk

9. AWNINGS AND BUILDING SECURITY DEVICES

Awnings

Awnings may encroach into the public right-of-way up to eight feet for the purpose of providing shade over the sidewalk, provided they are at least 10 feet above sidewalk grade.

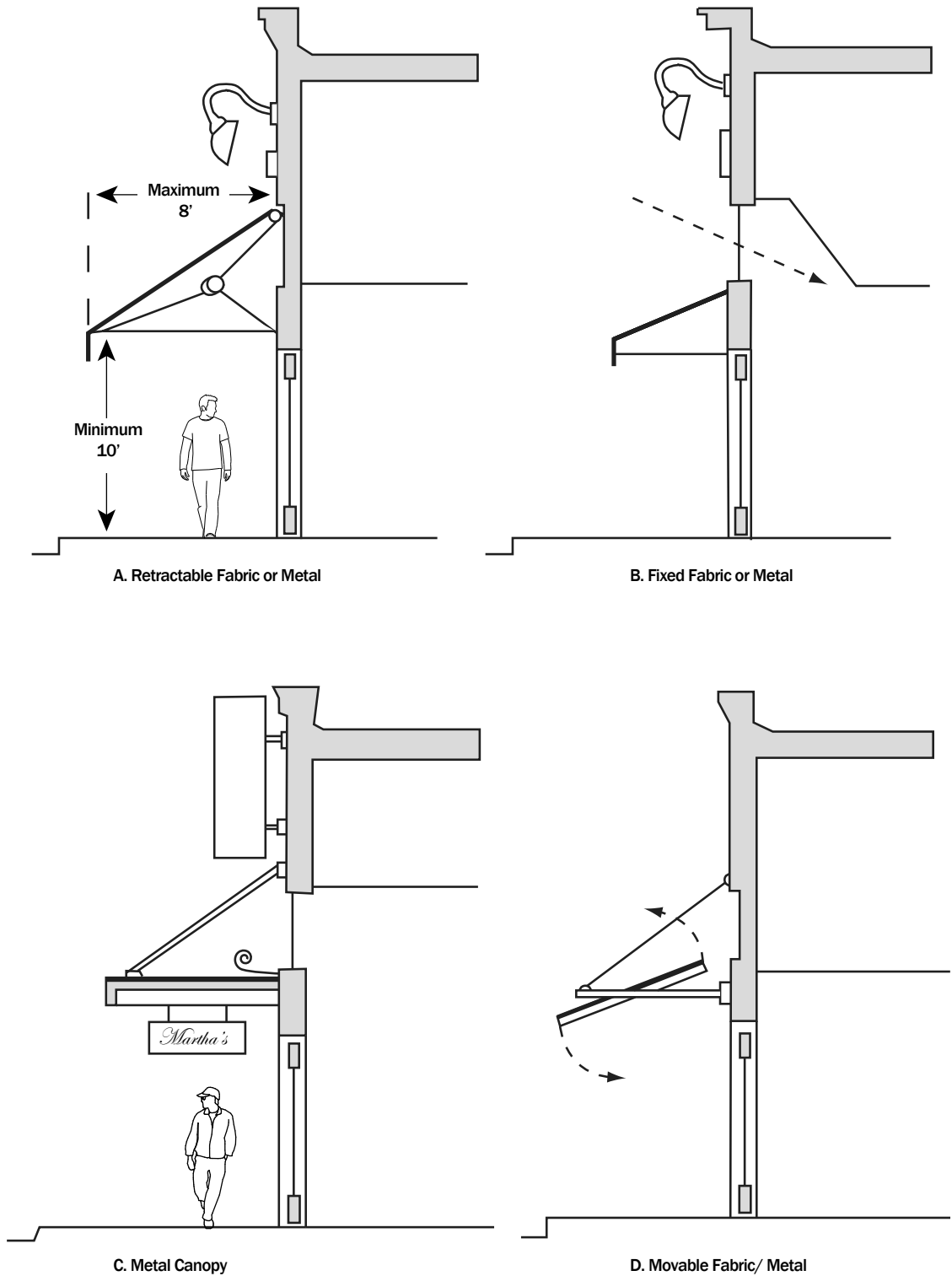
- *Type:* Awnings and canopies shall be retractable fabric or metal, fixed fabric or metal, or metal canopy.
- *Slope:* All awnings shall be sloped and flat. No awning shall be stretched over a curved frame unless photographic evidence can be shown that the building originally had such an awning configuration.
- *Colors:* Canvas awnings may be colored in darker hues, either as solid colors of deep olive, hunter green, deep red, burgundy, or old gold, or striped in combinations of two of these colors or one of these colors with white, cream or tan. Metal canopies should utilize aluminum anodized in a dark, neutral color or natural metal.
- *Placement:* May be attached above or below the transom window. To diminish the effects of late afternoon sunlight, buildings on the east side of north-south streets should consider attaching the awning above the transom window. Metal canopies shall be placed below the transom window.
- *Lighting:* Light fixtures placed under an awning may be used to light a projecting sign that may hang from the awning frame, or it may be employed to further illuminate the sidewalk or building entrance below. Concealed direct and accent lights can be placed over metal canopies to illuminate the front façade and and/or related front façade signs.

Building Security Devices

Where security grilles are required or otherwise provided, they shall be painted to match the trim colors of the building. Unobtrusive roll-down grilles with thin vertical and horizontal elements are preferred; heavy steel and accordion fold grates are discouraged.

Grilles, security doors, and other security devices designed for nighttime security should not be visible to the public during business hours or when they are in an open position.

Figure B9-1:
Awnings and Canopies



10. UTILITIES SCREENING

Utility meters and related equipment shall be located only on the side or rear façade of buildings and shall be integrated into the building architecture through painting or other means.



Utilities accessible along the side of buildings.



Utilities accessible from the rear alley on the side facade.

11. ROOFTOP MECHANICAL EQUIPMENT SCREENING

Rooftop mechanical equipment shall be screened from view from eye level on public streets, and the screening shall be designed as an integral component of the architectural design.



Appropriate screening of rooftop mechanical equipment includes location of equipment at the rear of the building.



Rooftop mechanical equipment not appropriately screened.

This page intentionally left blank

C. PARKING

Figure C-1:
Parking Lot Landscaping

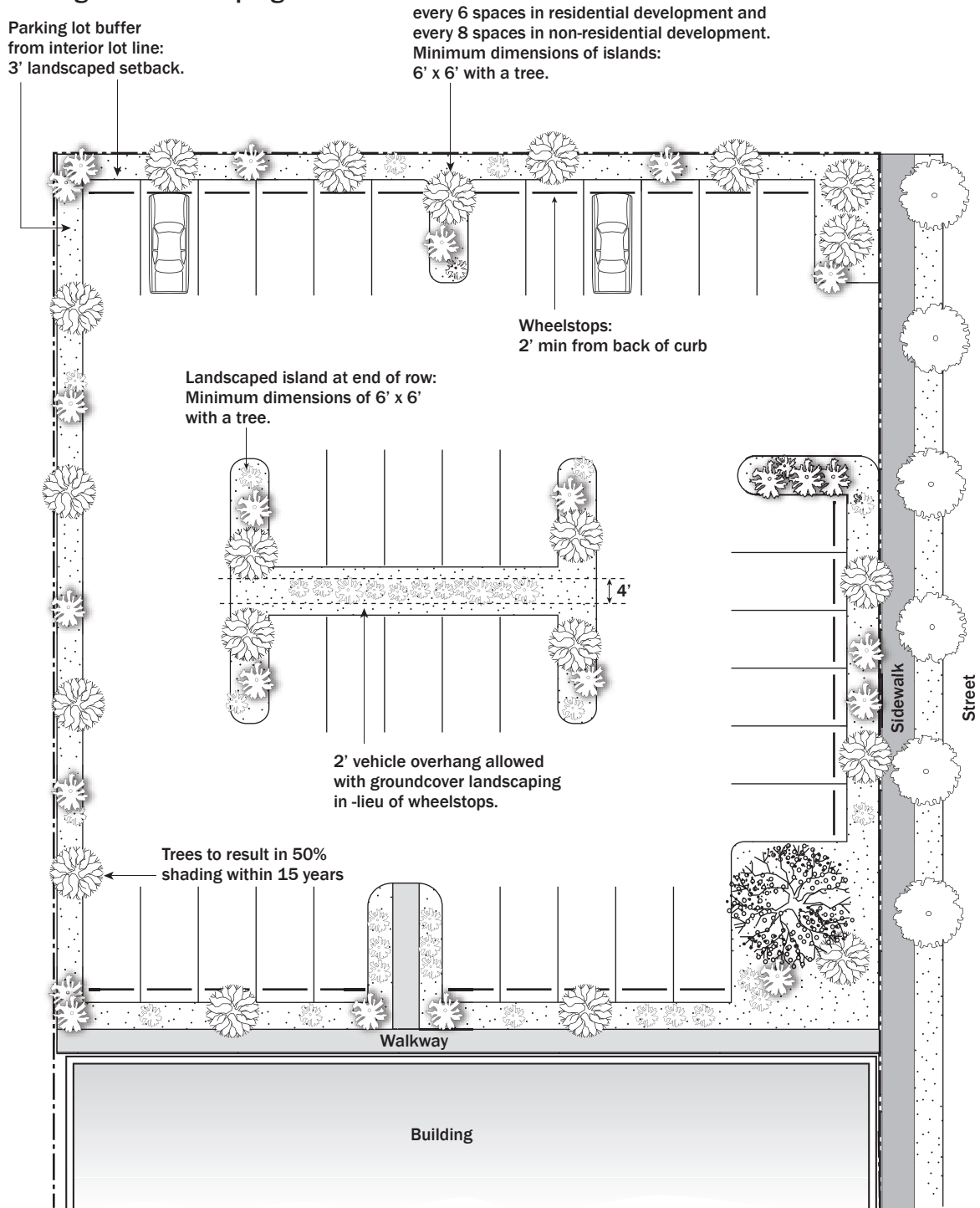
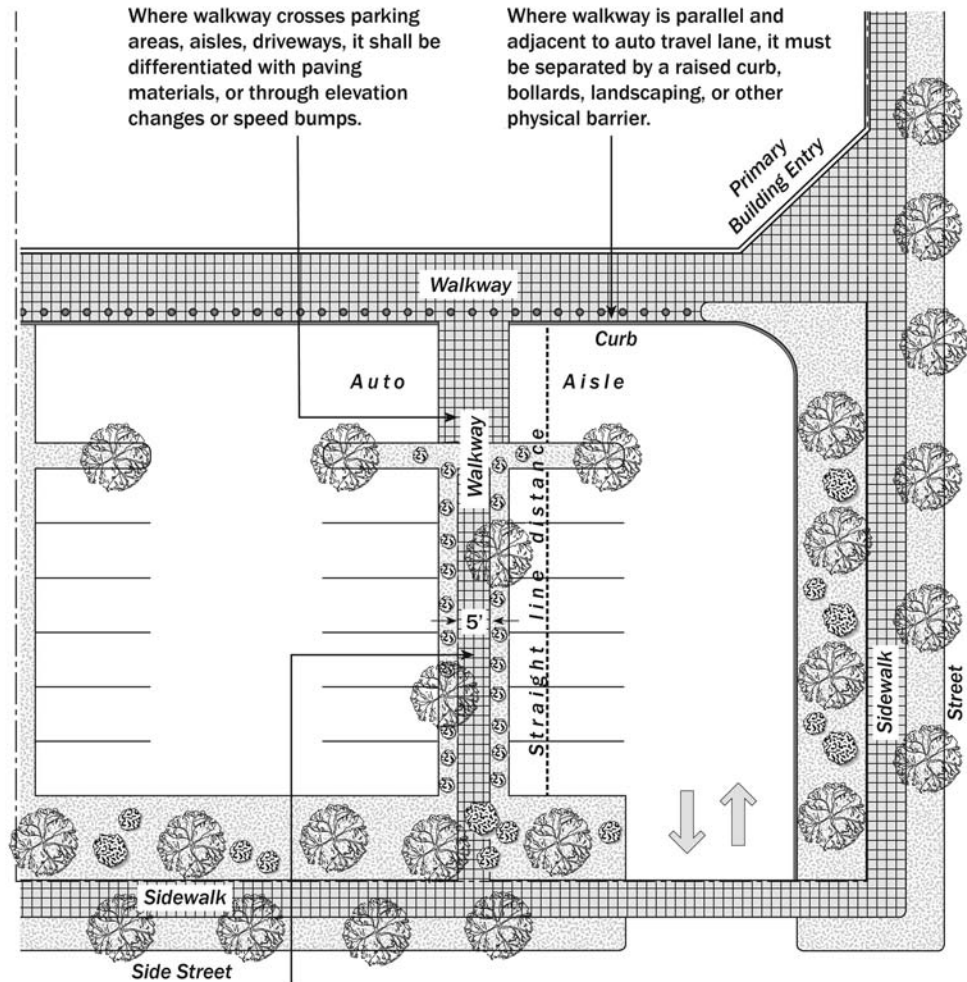


Figure C-2:
On-Site Pedestrian Walkways



An on-site walkway shall connect the primary building entry to a public sidewalk on each street frontage. Walkway shall be shortest practical distance between building entry and sidewalk, generally no more than 125% of the straight line distance.

D. SIGNS

Signs should:

- Be designed as an integral design element of a building's architecture, including color and scale, and compatible with any building to which the sign is to be attached and with surrounding structures.

Figure D-1:
Front Yard Signs

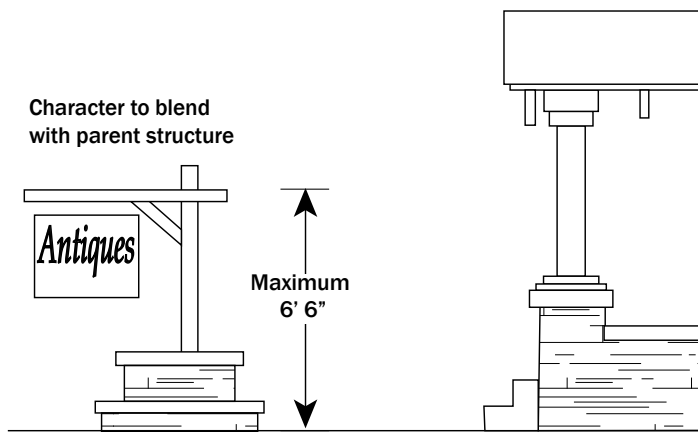


Figure D-2:
Window Sign

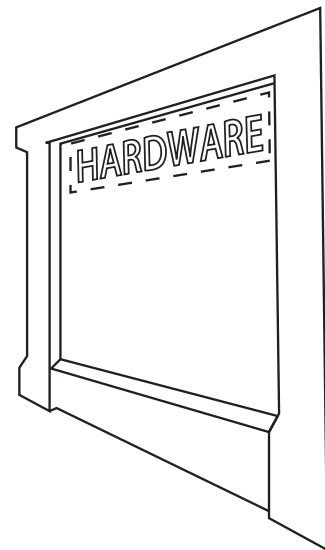


Figure D-3:
Front Façade Upper Transom Signs

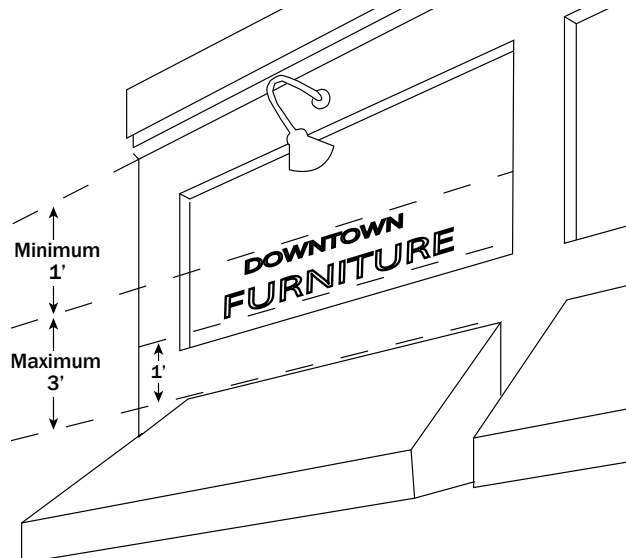
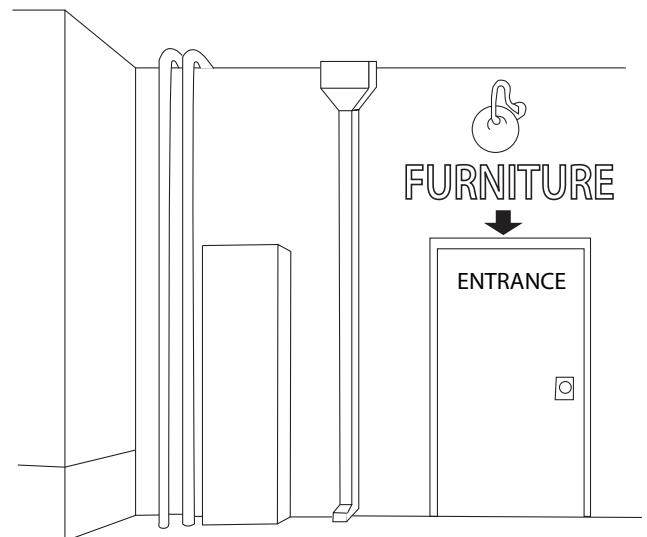


Figure D-4:
Rear and Side Façade Signs





- Be consistent with distinct area or district characteristics and incorporate common design elements such as sign materials or themes.
- The size and proportion of the elements of the sign's message should be selected based on the anticipated viewer.
- Have finished edges with a clean, smooth, consistent surface. Lettering on the sign is to be of complementary size, proportion and font and either carved, routed, painted or applied.
- Be conspicuous and readily distinguishable from their surroundings.

Figure D-5:
Rear and Side Façade Signs

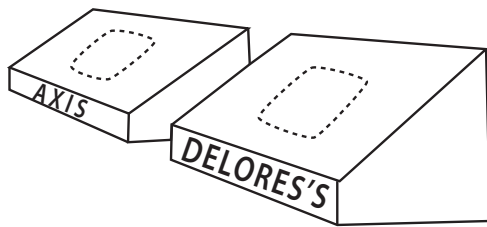


Figure D-6:
Monument Signs

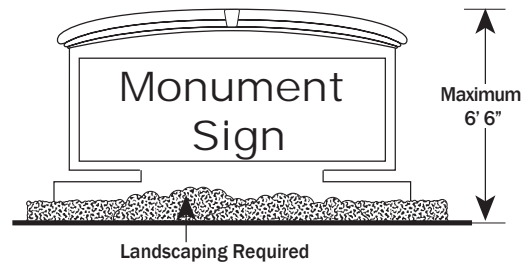


Figure D-7:
Projecting Blade Signs

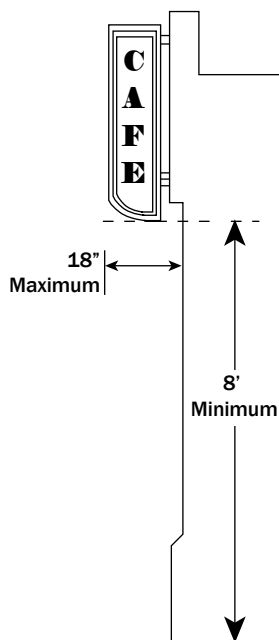
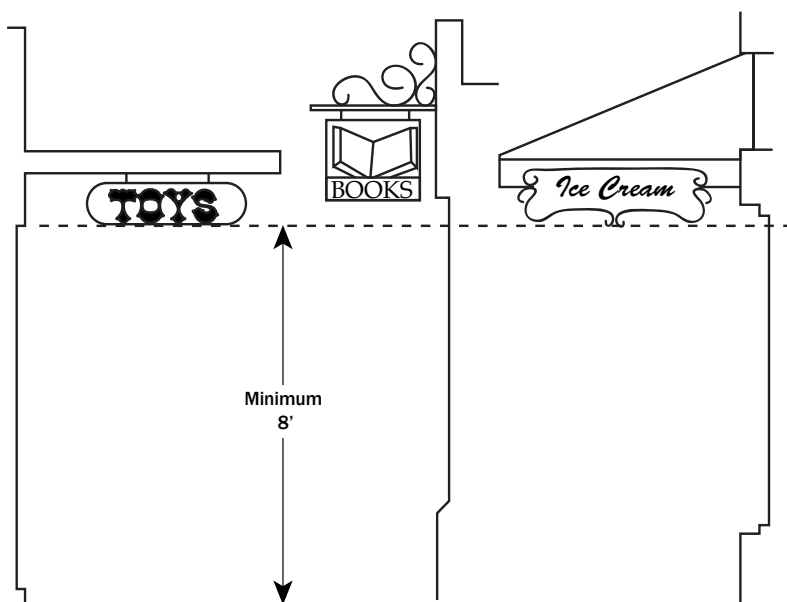


Figure D-8:
Guild Signs



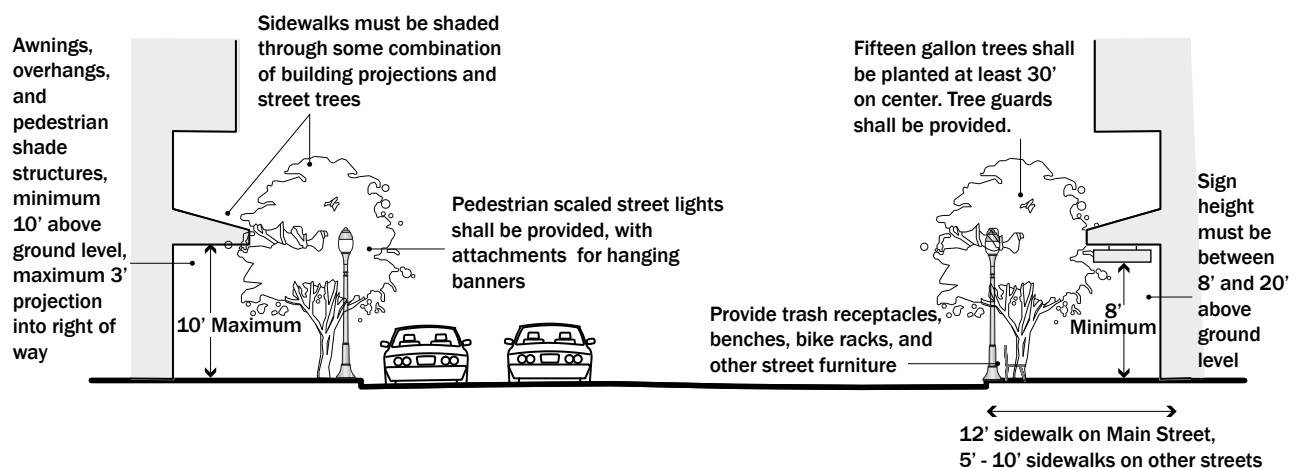
E. STREET IMPROVEMENTS

Public Improvements

Any new commercial or multi-family buildings or structural alterations and additions to commercial or multi-family buildings with a combined building permit valuation of \$17,544, adjusted annually based on Engineering News Records analysis, over a two year period, shall provide public improvements between the building and the curb in accordance with the following:

- *Sidewalks.* Sidewalks shall be provided if none already exist or if the existing sidewalks are in poor condition.
- *Street Furniture.* Trash receptacles, benches, bike racks, and other street furniture from a list maintained by the City Planning Department shall be provided.
- *Street Lights.* Pedestrian scaled street lights (12-18 feet tall), including attachments from which banners may be hung, from a list maintained by the City Planning Department shall be provided.
- *Street Trees.* Shade trees shall be planted at least 30 feet on center. Tree guards shall be provided. Trees shall be a minimum of 15 gallons in size. A minimum of 10 percent of the required trees shall be 24 inch box size or larger.
- *Sidewalk Shade.* A minimum of 50 percent of the sidewalk shall be shaded by a double row of trees, or a building projection (awning, overhang, or pedestrian shade structure) on one side and a street tree on the other side. In areas where 100 percent of the building is to be at the property line, this requirement can only be met by a building projection on one side and a street tree on the other side. If trees are used, the required amount of shading must be achieved within 15 years.

Figure E-1:
Pedestrian Amenities



Block Standards

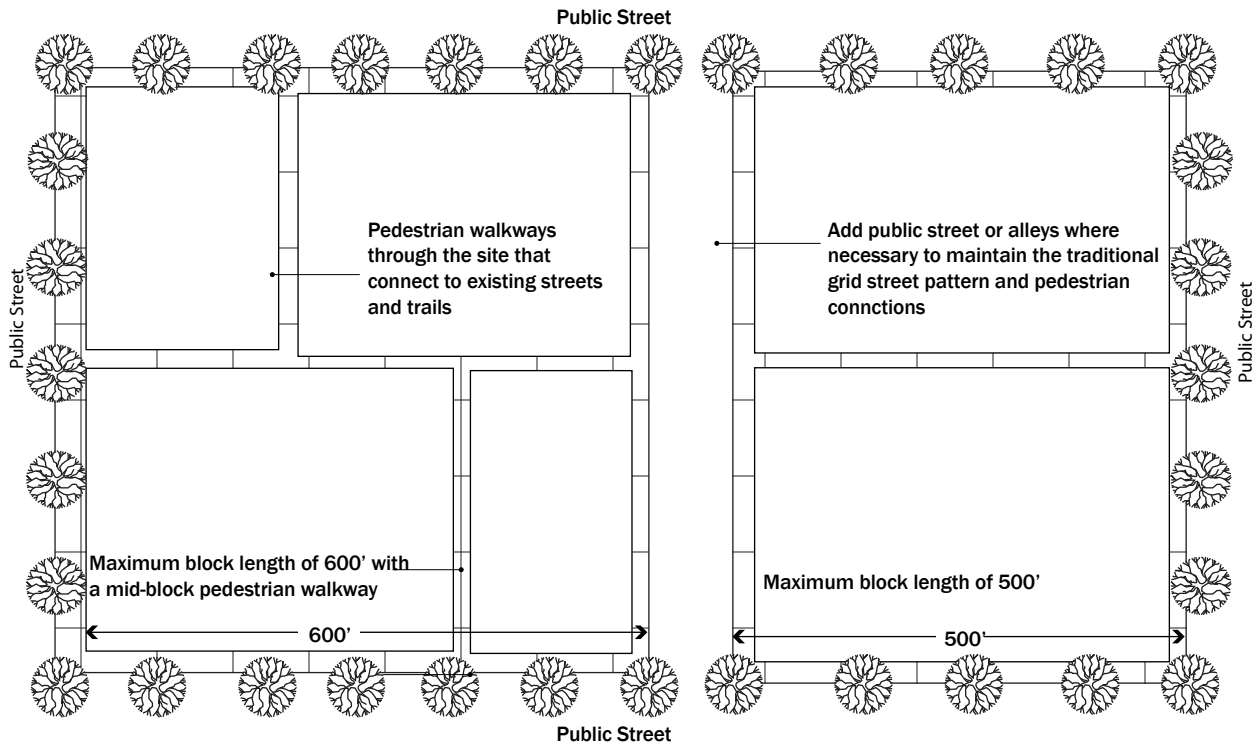
Maximum block dimension for projects on lots great than two acres in size:

- Maximum Block Length is 500 feet, measured curb edge to curb edge
- A block length of up to 600 feet shall only be allowed when a mid-block pedestrian connection is provided
- Maximum Block Size: Three acres.

Street Preservation

The Downtown street pattern creates the pedestrian-oriented character of Downtown, with short, direct walking routes between homes, offices, shops, and restaurants. Existing streets in the Downtown shall be preserved. Public right-of-way shall not be eliminated or abandoned unless substantial public benefits are provided, such as a new park.

Figure E-2:
Street Preservation



Medians, Parkways, and Other Public Projects

The implementation of street design and improvements as described in the Design Guidelines may be accomplished through a program run by the City and/or in partnership with private development. Such improvements may include the installation of street trees, street lights, street furniture, and medians on Main Street between Olive Avenue and River Gateway and on D Street between Putnam Avenue and Olive Avenue.

This page intentionally left blank

F. GLOSSARY

Abutting or Adjoining. Having a common border, boundary, or lot line. Lots or parcels of land that touch at corners only shall not be deemed adjoining.

Acre, Gross. A measure of total land area of a lot or site, including areas to be dedicated for public rights-of-way, streets, schools, or other dedications.

Acre, Net. A measure of land area of a lot or site remaining after dedication of all areas for public rights-of-way, streets, schools, or other dedications.

Adjacent. Directly abutting, having a boundary or property line(s) in common or bordering directly, or contiguous to.

Alley. A public way permanently reserved primarily for secondary vehicular service access to the rear or side of properties otherwise abutting on a street.

Alteration. Any change, addition or modification that changes the exterior architectural appearance or materials of a structure or object. Alteration includes changes in exterior surfaces, changes in materials, additions, remodels, demolitions, and relocation of buildings or structures, but excludes ordinary maintenance and repairs. *See also "Structural Alterations" for modifications to any of the supporting members of a structure.*

Area, Gross. The horizontal area within the boundaries of a lot or site including any area for future streets, parks, and other dedications.

Balcony. A platform that projects from the wall of a building 30 inches or more above grade.

Block. The frontage along one side of a street between the two nearest intersecting streets.

Building. Any structure having a roof supported by columns or walls and intended for the shelter, housing or enclosure of any individual, animal, process, equipment, goods, or materials.

Building Front. That portion of the main building which affords public entry. In the case of a building with more than one public entry, the entrance with the assigned address shall be considered the main public entry.

Building Height. *See Height.*

Building, Main. A building in which is conducted the principal use of the lot on which it is situated.

City. The City of Porterville.

City Council. The City Council of the City of Porterville.

Compatible. That which is harmonious with and will not adversely affect surrounding buildings and/or uses.

Conditionally Permitted. Permitted subject to approval of a Conditional Use Permit or Site Plan Review.

Construction. Construction, erection, enlargement, alteration, conversion or movement of any building, structures, or land together with any scientific surveys associated therewith.

D Districts (Downtown Districts). One or more of the following districts: DR-N Downtown Retail – North of Olive Avenue, DR-S Downtown Retail – South of Olive Avenue, DR-D Downtown Retail – D Street Corridor, D-MX Downtown Mixed-Use, D-PO Downtown Professional Office, D-CG Downtown General and Service Commercial, D-PS Downtown Public and Semi Public, DRM-2 Downtown Medium Density Residential, and DRM-3 Downtown High Density Residential.

Deck. A platform, either freestanding or attached to a building, that is supported by pillars or posts. *See also “Balcony.”*

Density. The number of dwelling units per unit of land area.

Development. Any manmade change to improved or unimproved real estate, including but not limited to the division of a parcel of land into two or more parcels; the construction, reconstruction, conversion, structural alteration, relocation, or enlargement of any structure; any mining, excavation, landfill or land disturbance; and any use or extension of the use of land.

Development Ordinance. The Development Ordinance of the City of Porterville, as amended.

District. *See Zoning District.*

Double Frontage Lot. *See Lot, Through.*

Driveway. An accessway composed of concrete or asphalt that provides vehicular access between a public street or legal right-of-way, controlled by the City of Porterville, and the parking or loading facilities located on an adjacent property.

Dwelling Unit. A room or suite of rooms including one and only one kitchen, and designed or occupied as separate living quarters for one family. *See also “Family.”*

Facade. The exterior wall of a building exposed to public view or that wall viewed by persons not within the building. The portion of any exterior elevation of a building extending vertically from the grade to the top of a parapet wall or eave, and horizontally across the entire width of the building elevation.

Family. One or more persons living together as a single nonprofit housekeeping unit and sharing common living, sleeping, cooking and eating facilities. Members of a “family” need not be related by blood but are distinguished from a group occupying a hotel, club, fraternity or sorority house.

Fence. An artificially-constructed barrier of any material or combination of materials erected to enclose or screen an area of land.

Feasible. Capable of being accomplished in a successful manner within a reasonable period of time, taking into account economic, environmental, social and technological factors.

Floor Area. The total floor area in a building (including basements, mezzanines, interior balconies, and upper stories or levels in a multistory building) unless otherwise stipulated; e.g., “ground” floor area.

Floor Area Ratio (FAR). The ratio of the total floor area of all buildings on a lot to the lot area.

Frontage, Street (Frontage, Lot). That portion of a lot or parcel of land that borders a public street. “Street frontage” shall be measured along the common lot line separating said lot or parcel of land from the public street, highway, or parkway.

Garage. A building or portion of a building that is enclosed and roofed and designed for the storage of motor vehicles.

General Plan. The Porterville 2030 General Plan.

Ground Floor. The first floor of a building other than a cellar or basement.

Height. The vertical distance from the highest point of any structure to the ground level directly below.

Intersection, Street. The area common to two or more intersecting streets.

Landscaping. The planting, configuration and maintenance of trees, ground cover, shrubbery and other plant material, decorative natural and structural features (walls, fences, hedges, trellises, fountains, sculptures), earth patterning and bedding materials, and other similar site improvements that serve an aesthetic or functional purpose.

Lot. A parcel, tract, or area of land whose boundaries have been established by a legal instrument such as a recorded deed or a recorded map, and which is recognized as a separate legal entity for purposes of transfer of title, except public easements or rights-of-way.

Lot Area. The total area circumscribed by the boundaries of a lot, excluding any street rights-of-way.

Lot Frontage. *See Frontage, Street.*

Lot Line. Any boundary of a lot.

Lot Line Types.

Front Lot Line. The lot line that abuts a street or public right-of-way. For corner lots, the shortest side fronting a public street is considered the front lot line regardless of which street is used for vehicle or pedestrian access, or street address.

Interior Lot Line. Any lot line that is not adjacent to a street.

Rear Lot Line. The lot line that is opposite and most distant from the front lot line. Where no lot line is within 45 degrees of being parallel to the front lot line, a line 10 feet in length within the lot, parallel to and at the maximum possible distance from the front lot line, will be deemed the rear lot line for the purpose of establishing the minimum rear yard.

Side Lot Line. Any lot line that is not a front or rear lot line.

Street Side Lot Line. A side lot line of a corner lot that is adjacent to a street.

Lot Types.

Lot, Corner. A lot or parcel situated at the intersection of two or more streets that have an angle intersection measured within said lot or parcel of not more than 135 degrees.

Lot, Interior. A lot or parcel of land other than a corner or flag lot.

Lot, Through. A lot having frontage on two parallel or approximately parallel streets.

Lot Width. The distance between the side lot lines, measured at a right angle to the lot depth at the midpoint of the lot depth line. See also Section 103.06, Measuring Lot Width and Depth.

On-Site Loading Facilities. A site or portion of a site devoted to the loading or unloading of motor vehicles or trailers, including loading berths, aisles, access drives, and landscaped areas.

On-Site. Located on the lot that is the subject of discussion.

Open Space Types.

Private Open Space. Open areas for outdoor living and recreation that are adjacent and directly accessible to a single dwelling unit, reserved for the exclusive use of residents of the dwelling unit and their guests.

Common Open Space. Areas for outdoor living and recreation that are intended for the use of residents and guests of more than one dwelling unit.

Usable Open Space. Outdoor areas that provide for outdoor living and/or recreation for the use of residents.

Owner. Any person owning property, as shown on the last equalized assessment roll for city taxes or the lessee, tenant or other person having control or possession of the property.

Parking Area. A lot, structure, or any other area, including driveways, which is designed for and the primary purpose of which is to provide for the temporary storage of vehicles.

Permit. Any Zoning Certificate, Conditional Use Permit, Temporary Use Permit, Building Permit, license, certificate, approval, or other entitlement for development and/or use of property as required by any public agency.

Person. Any individual, firm, association, organization, partnership, business trust, company, or corporation.

Project. Any proposal for a new or changed use or for new construction, alteration, or enlargement of any structure, that is subject to the provisions of this Ordinance. This term also refers to any action that qualifies as a “project” as defined by the California Environmental Quality Act.

Right-of-Way. A strip of land acquired by reservation, dedication, forced dedication, prescription or condemnation and intended to be occupied or occupied by a road, railroad, electric transmission lines, oil or gas pipeline, water line, sanitary storm sewer or other similar use.

Screening. Screening refers to a wall, fence, hedge, informal planting, or berm, provided for the purpose of buffering a building or activity from neighboring areas or from the street.

Setback. The area between a property line and a building or structure which must be kept clear or open except as otherwise permitted by the Development Ordinance.

Sidewalk. A paved, surfaced, or leveled area, paralleling and usually separated from the street, used as a pedestrian walkway.

Sign. Any letters, figure, visual symbol, emblem, logo, object, or display, or any combination thereof, designed or used to identify, attract attention to, advertise, or communicate information. Signs include display surfaces together with such facilities as are utilized in supporting, maintaining, and illuminating the display surfaces.

Sign Height. The vertical distance measured from the lowest ground level directly beneath the sign to the highest point at the top of the sign. The ground level shall be either the natural grade or finished grade, whichever is lowest.

Site. A lot, or group of contiguous lots, that is proposed for development in accordance with the provisions of this Ordinance and is in a single ownership or under unified control.

Story. That portion of a building included between the upper surface of any floor and the upper surface of the floor next above, except that the topmost story shall be that portion of a building included between the upper surface of the topmost floor and the roof above.

Street. A public or private right-of-way whose function is to carry vehicular traffic or provide vehicular access to abutting property.

Structural Alteration. Any change of the supporting members of a building, such as bearing walls, columns, beams or girders, floor joists, ceiling joists, or roof rafters.

Tandem Parking. An arrangement of parking spaces such that one or more spaces must be driven across in order to access another space or spaces.

Unit. *See Dwelling Unit.*

Use. The purpose for which a site or structure is arranged, designed, intended, constructed, erected, moved, altered, or enlarged for which either a site or a structure is or may be occupied or maintained.

Visible. Capable of being seen (whether or not legible) by a person of normal height and visual acuity walking or driving on a public road.

Yard. An open space on a lot that is unoccupied and unobstructed from the ground upward, except as otherwise permitted by the Development Ordinance.

Front Yard. A yard extending across the front of a lot for the full width of the lot between the side lot lines. The depth of a front yard shall be a distance specified by the Development Ordinance for the district in which it is located and measured inward from the front lot line.

Interior Side Yard. A yard extending along an interior side of a lot from the front lot line to the rear lot line, and to a depth specified by the Development Ordinance for the district in which it is located and measured inward from the interior side lot line.

Street Side Yard. A yard extending along the street side of a corner lot from the front lot line to the rear lot line, and to a depth specified by the Development Ordinance for the district in which it is located and measured inward from the street side lot line.

Rear Yard. A yard extending across the rear of a lot for its full width between side lot lines, and to a depth specified by the Development Ordinance for the district in which it is located. If a lot has no rear lot line, a line 10 feet in length within the lot, parallel to and at the maximum possible distance from the front lot line, will be deemed the rear lot line for the purpose of establishing the minimum rear yard.

Zoning Administrator. The Community Development Director of the City of Porterville, or his or her designee.

Zoning District. A specifically delineated area or district in the city within which regulations and requirements uniformly govern the use, placement, spacing, and size of land and buildings.

DYETT & BHATIA
Urban and Regional Planners

755 Sansome Street, Suite 400
San Francisco, California 94111
☎ 415 956 4300 📠 415 956 7315